



CITY OF SAINT ANTHONY VILLAGE
PLANNING COMMISSION MEETING AGENDA
FEBRUARY 26, 2018
COUNCIL CHAMBERS
7:00 PM

Call to Order.

Pledge of Allegiance.

Roll Call.

Consideration, discussion, and possible action on all of the following items:

I. Approval of the February 26, 2018 Planning Commission Meeting Agenda. (action requested)

II. Approval of the October 23, 2017 Regular Planning Commission Meeting Minutes.

III. Public Hearing.

A. Consider a request from The Village, LLC for the following land use applications at 2501 Lowry Avenue

- Request for a Planned Unit Development (PUD) preliminary development plan review and Re-Zoning Request.
- Request for Preliminary Plat for The Village, LLC

IV. Staff Reports.

V. Other Business.

VI. Community Forum.

Individuals may address the City Council about any item not included on the regular agenda.

Speakers are requested to come to the podium, sign their name and address on the form at the podium, state their name and address for the Clerk's record, and limit their remarks to five minutes. Generally, the City Council will not take official action on items discussed at this time, but may typically refer the matter to staff for a future report or direct the matter to be scheduled on an upcoming agenda.

VII. Information and Announcements.

VIII. Adjournment.

If you would like to request special accommodations or alternative formats, please contact the City Clerk at 612-782-3313 or email city@savmn.com. People who are deaf or hard of hearing can contact us by using 711 Relay.

Our Mission is to be a progressive and livable community, a walkable village, which is sustainable, safe and secure.

**CITY OF ST. ANTHONY
PLANNING COMMISSION MEETING
October 23, 2017
7:00 p.m.**

CALL TO ORDER.

Chairperson Gondorchin called the meeting to order at 7:00 p.m.

PLEDGE OF ALLEGIANCE.

Chairperson Gondorchin invited the Commission and the audience to join him in the Pledge of Allegiance.

ROLL CALL.

Commissioners Present: Chairperson Gondorchin, Commissioners Foster, Kalar, Larson, and Papatola.

Absent: Commissioners Bartel and Westrick

Also Present: City Planner Breanne Rothstein

I. APPROVAL OF THE OCTOBER 23, 2017 PLANNING COMMISSION MEETING AGENDA.

Motion by Commissioner Foster, seconded by Commissioner Kalar, to approve the October 23, 2017 Planning Commission agenda.

Motion carried 5-0.

II. APPROVAL OF THE SEPTEMBER 25, 2017 REGULAR PLANNING COMMISSION MEETING MINUTES.

Commissioner Papatola noted on page 4 line 30 the speaker's name is Perry Thorvig.

Motion by Commissioner Papatola, seconded by Commissioner Kalar, to approve the September 25, 2017 Regular Planning Commission Meeting Minutes as amended.

Motion carried 5-0.

Commission Kalar will represent the Planning Commission at the November 14, 2017 City Council Meeting.

III. PUBLIC HEARING.

A. Consider a request for a conditional use permit to allow an adult daycare facility on the property located at 2400 37th Avenue.

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1
2 Chairperson Gondorchin opened the public hearing at 7:03 p.m.
3

4 City Planner Breanne Rothstein reviewed the application submitted by Isse Hussein, requesting a
5 Conditional Use Permit (CUP) to allow an adult day-care center located at 2400 37th Avenue NE.
6 The request is consistent with the intent of the Zoning Ordinance and the City's Comprehensive
7 Plan. The property owner is Sadiq Punjani. The existing land use zoning is Commercial/Zoned
8 C – Commercial. The prior business on the property was a gas station. Ms. Rothstein provided a
9 description of the request.
10

11 Ms. Rothstein reviewed the CUP Criteria review:
12

13 -Use is one of the conditional uses specifically listed for the district in which the property is
14 located: Criterion Met.
15

16 -Use will not be detrimental to the health, safety or general welfare of persons residing or
17 working in the vicinity: Criterion Met
18

19 -Use will provide a service or facility which is in the interest of public convenience and will
20 contribute to the general welfare: Criterion Met.
21

22 Staff is recommending approval of the application, subject to the following conditions:
23

24 -The Applicant shall submit the appropriate plans and permits for review and approval before
25 beginning any construction or other alterations that requires a permit (i.e. building permit for any
26 interior work, sign permit, electrical permit, plumbing permit, etc.).
27

28 -Applicant receives a revised SAC determination, per the Met Council.
29

30 Chair Gondorchin stated he reviewed the MN Statute 245A-14 Family Adult Day Services. The
31 Applicant is proposing 40 residents attending the facilities which would allow for 36 square feet
32 per person. The Statute allows for 35 square feet per person not including staff. Ms. Rothstein
33 stated they have not evaluated if this request is consistent with the permit or not as that is the
34 Department of Health's responsibility. The City does require a State Permit which would cover
35 all the requirements of the State for such a facility. Chair Gondorchin asked how many staff
36 personnel are required per attendee and the Statute states one adult may supervise up to eight
37 attendees. Chair Gondorchin asked if there were attendees with special needs are more
38 supervisory staff required. He also asked about the training and skill levels of the staff.
39

40 Commissioner Foster asked if the underground storage tanks from the gas station were removed
41 and was remediation done. He asked if there are any environmental issues that may be present.
42 Ms. Rothstein stated she believes the tanks were removed. She will have the Applicant confirm
43 that.
44

45 Commissioner Papatola asked if the Applicant is requesting renovation of the current structure
46 and not constructing a new structure. Ms. Rothstein stated that was correct.

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1
2 The Applicant stated all tanks have been removed per the MN Department of Pollution Agency.
3 He described the process and noted all the work was completed and the property was given a
4 clean bill of health.
5

6 Mr. Isse Hussein, the applicant, responded to the questions posed. The County and State both
7 have many requirements. They have in place nurse, nutritionist, therapists, etc. Some of the
8 attendees will have one to one care. Chair Gondorcin asked how many staff members are
9 currently in place. Mr. Hussein stated so far there are four. All food will be catered but they have
10 a nutritionist. He stated all personnel are licensed by the State.
11

12 Commissioner Papatola asked Mr. Hussein about the permitting process. The Applicant stated
13 there are building permits required. Safety is the number one concern when designing an adult
14 day care. The State has a lot of stringent guidelines. The State comes and checks to make sure all
15 items are aligned. The State determines how many people can attend. The license is issued after
16 everything is checked. The square feet will be 1700.
17

18 Chair Gondorcin asked how many clients was the financial business criteria estimating. The
19 Applicant stated 17-21 would provide a good money-making operation. Low 20's would be the
20 cutoff point.
21

22 Mr. Perry Thorvig asked the applicants to explain more about the hours of operation, the
23 population they plan to serve, how will clients get to the center and what will the client be
24 charged. He also asked what is meant by "taking care of them".
25

26 Chair Gondorcin noted the documentation states the business will be operated six hours per
27 day. The Applicant corrected the hours of operation will be from 9:00 a.m. to 9:00 p.m. normal
28 operating hours or 9:00 a.m. – 8:00 p.m. There was a typo when stating six hours per day. It will
29 be open to everybody. He reviewed the goals and activities offered to make the client feel they
30 are contributing. Chair Gondorcin asked if they have any other facilities they operate and if so
31 what hours are those facilities. Mr. Hussein stated the normal hours are six hours a day and they
32 would be open from 9:00 a.m. to 7:00 p.m. The staff would only work 6-7 hours per day. Mr.
33 Hussein stated they offer senior care but not in a day care situation. Ms. Rothstein stated the
34 Commission can establish specific hours of operation in their recommendation. She would
35 recommend 9:00 a.m. to 7:00 p.m. which is consistent with other adult day care facilities she is
36 familiar with. Chair Gondorcin stated Mr. Thorvig had asked what "taking care" means.
37

38 The Applicant stated some of the clients may need special care and would receive one on one
39 care. Others would have one employee per eight clients. Commissioner Papatola asked if there
40 are specific standards of care mandated by the County or State. The Applicant stated there are
41 standards of care. All requirements will be met.
42

43 Chair Gondorcin asked if there were five dementia clients would they each receive one on one
44 care. Mr. Hussein stated he is not the one that would make some of the decisions. Each client
45 will have a care plan which the social worker or nurse would develop. The Applicant stated the
46 conditions/criteria will be in place and modified as to the number of s and clients that will be in

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1 the building. The Department of Social Services is very stringent and will help determine the
2 care plans. The insurance companies are very stringent also. Chair Gondorchin asked about the
3 transportation plan. The Applicant stated in most cases individuals will be brought in by
4 someone or Mr. Hussein stated they will provide transportation.
5

6 Commissioner Kalar stated through his job he deals with adult day cares. The center must always
7 have the correct ratio and it is part of their business plan. The ramification is loss of license. He
8 is confident the applicant understands what the requirements are and will strive to meet them.
9

10 Chair Gondorchin closed the public hearing at 7:40 p.m.
11

12 Motion by Chairperson Kalar, seconded by Commissioner Foster, to recommend the City
13 Council approve a conditional use permit (CUP) to allow an adult day-care center, licensed
14 under M.S. Chapter 245A, and Minn. Rules at 2400 37th Avenue NE.
15

16 **Motion carried 5-0.**
17

18 **IV. STAFF REPORTS – NONE.**
19

20 **V. OTHER BUSINESS – NONE.**
21

22 **VI. COMMUNITY FORUM.**
23

24 Mr. Perry Thorvig asked about Lowry Grove. He heard today there is an RV parked on the
25 premises. He has heard about the re-establishment of a non-conforming use which is extremely
26 rare. He asked if the use is re-established will the City require adequate or new infrastructure on
27 the property prior to the time any seasonal RV's or mobile homes are moved onto the site. Ms.
28 Rothstein stated it would depend on what the City Attorney advises.
29

30 **VII. INFORMATION AND ANNOUNCEMENTS – NONE.**
31

32 **VIII. ADJOURNMENT.**
33

34 Motion by Commissioner Papatola, seconded by Commissioner Foster, to adjourn the meeting at
35 7:45 p.m.
36

37 **Motion carried 5-0.**
38

39
40 Respectfully submitted,
41 Debbie Wolfe (*TimeSaver Off Site Secretarial, Inc.*)



MEMORANDUM

To: St. Anthony Village Planning Commission
From: Breanne Rothstein, AICP, City Planner
Date: Planning Commission meeting on February 26, 2018
WSB Project No. 02170-300
Request: Preliminary PUD Development Plan and Preliminary Plat for The Village, LLC

GENERAL INFORMATION

Applicant: The Village, LLC
Owners: The Village, LLC
Location: 2501 Lowry Avenue
Existing Land Use: Vacant
Zoning: R-1 Single-Family Residential

Surrounding Land Use / Zoning: North: Low Density Residential / Zoned R-1 - Single Family Residential
East: High Density Residential/Planned Unit Development Kenzington Terrace (ownership condos for seniors) are located immediately to the east (5 stories, plus at grade garage (70 feet in height).
South: High Density Residential and Commercial/Planned Unit Development The Legacy (4 stories of rental, assisted living for seniors (48 feet), Walker Senior Housing (3 stories of affordable rental, assisted living for seniors (36 feet), and Autumn Woods (3 stories general occupancy rental units (40 feet).
West: Single-Family Residential and Commercial in Minneapolis

BACKGROUND

The applicant for the proposal, The Village, LLC, with lead developer Continental Property Group, purchased the Lowry Grove Manufactured Home Community on June 13, 2016. In the ensuing year, the mobile home park, and the former and new property owners engaged in a series of State statutory requirements for mobile home park closures. The Lowry Grove mobile home park formally closed on June 30, 2017.

On October 24, 2016, the Planning Commission reviewed an initial Planned Unit Development (PUD) sketch plan submission for the site from The Village, LLC. An Environmental Assessment Worksheet (EAW) for the site was completed in November of 2016.

On July 18, 2017, The Village, LLC submitted a series of land use applications for their proposed redevelopment of the former Lowry Grove site that also included Bremer Bank. On October 10, 2017, the City Council denied the request and adopted a resolution outlining their stated reasons.

On December 22, 2017, The Village, LLC submitted new applications and requests for a preliminary PUD plan/rezoning, a preliminary plat, and a comprehensive plan amendment for the site. Their submission was deemed incomplete at that time, due to changes in status of the purchase agreement over the Bremer property.

On February 8, 2018, The Village, LLC submitted revised plan for preliminary plat and preliminary PUD development plan for the site that excluded the Bremer Bank site. The application was deemed complete by the city and is the subject of this review.

DESCRIPTION OF THE REQUEST

The Village, LLC has submitted the following requests with their land use application for the redevelopment plan for 2501 Lowry Avenue:

- **Planned Unit Development (PUD) preliminary development plan review and Re-Zoning Request.**
- **Preliminary Plat review for their re-development plan for 2501 Lowry Avenue;** a preliminary plat submission must include certain required elements to be considered for complete review.

A detailed narrative from the developer was submitted and included as an attachment to this staff report.

The site under re-development consideration is approximately 15.4 acres located on the southwestern edge of the city of St. Anthony at 2501 Lowry Avenue. The preliminary plat and PUD preliminary development submittal proposes a residential redevelopment that includes several housing types and styles; independent senior rental housing, assisted living senior housing, and general occupancy rental housing. There are a total of 430 housing units proposed on the site, comprised of three buildings of multifamily housing. While the floor plans currently show 414 units, the developer is requesting the ability to re-configure the floor plans and unit sizes and number within the current building footprints proposed, up to a hard cap of 430 units. This results in 28 units per acre.

The independent senior building (Building A) is proposed to include approximately 61 units of studios, 1 bedrooms and 2 bedrooms and is 45.5 feet in height. Building B, consisting of assisted living senior housing, is proposed to have approximately 128 units, which range in size from very small, memory care units to larger 2 bedroom units. This building is proposed to be 45.5 feet in height. The general occupancy rental building, Building C is proposed to have a mix of studio (approximately 77), 1 bedrooms (100), 2 bedrooms (36) and 3 bedrooms (12). Building C is proposed to be 44 feet in height. These exact breakdowns are subject to change, based on final market studies and plans.

There are a total of 693 designated parking stalls proposed for the development. These include 407 enclosed parking stalls and 286 surface stalls. The redevelopment proposal includes two stormwater retention ponds proposed at grade. The City Engineer has reviewed the surface water management plan and submitted comments in the attached memo. Trails are proposed throughout the development to connect with existing sidewalks and to provide inter-site recreational opportunities.

The plan proposes to connect to the existing sewer, water, and stormwater facilities located under Kenzie Terrace and Stinson Parkway.

ZONING GUIDANCE

The site is currently zoned **R-1 – Single Family**, which is a zoning district that allows for detached single family uses. The applicant is requesting to rezone the site to **Planned Unit Development (PUD)**. The intent of a PUD zoning designation is “to create a more flexible, creative, and efficient approach to the use of land”, and may include a greater use variety and dimensional flexibility than the underlying zoning district(s) would allow.

City Code section §152.203 (“ALLOWED USES”) stipulates that a PUD development plan may deviate from the area’s underlying zoning and allowed uses, and that the uses defined in the final PUD agreement take precedence:

“Uses within a PUD may include only those uses generally considered associated with the general land use category shown for the area on the official Comprehensive Land Use Plan. However, in some unique situations, the PUD may allow the approval of use or uses that are not listed as either permitted or conditional uses in any underlying zoning district. The specific allowed uses and performance standards for each PUD shall be delineated in an ordinance and development plan. The PUD development plan shall identify all the proposed land uses, which shall become permitted uses if the final development plan is approved.”

Upon review of the proposal, the only deviation found from the standards of the R-4 Multiple Family Residential district (the appropriate zoning district to review this property under, per the Comprehensive Plan Guidance of Multi-Family Residential) is the height of the buildings. Maximum height in the R-4 zoning district is 35 feet, and the maximum height of the proposed highest building is 45.5 feet.

PRELIMINARY PUD DEVELOPMENT REVIEW

Staff deemed the preliminary plat submission complete after reviewing the submitted elements against the required information listed in §151.02. The following summarizes the proposal for each element of the preliminary plat and preliminary PUD site plan.

Housing Types and Massing/Height: The preliminary plat and PUD preliminary development submittal proposes a residential redevelopment that includes multi-family, general occupancy housing, senior independent living, and senior assisted living. The table below summarizes the housing types and massing for each segment of the proposed development.

Site Element	Housing Type	Total Units	Height
Building A	Senior independent housing	61	3 story peaked roof, 31'5" to main eave, 45.5 feet total

Building B	Senior housing	128	3 story peaked roof 31'5" to main eave, 45.5 feet total
Building C	Market-rate	225	4 story flat roof, 44' total
Total Units Proposed: 414 housing units on 15.37 acres			
Overall Site Density: 26.95 units per acre			

Buildings A and B, both proposed at three stories in height, are located north of the roadway that runs through the site. Building C is proposed to have four stories, and fronts Kenzie Terrace, south of the proposed on-site roadway. Buildings A and B are proposed to be managed and operated by Ebenezer, a senior home community. Building C is proposed to be constructed and operated by Continental Property Group. The density proposed meets the Comprehensive Plan guidance for Multi-Family Residential properties.

Traffic, Streets and Access: The development plan proposes two main access points, one from Stinson Boulevard and one from Kenzie Terrace. The roadway is included on the preliminary plat as Outlot A and is proposed as private roadways. Access from the alley has been eliminated in the revised plan. A traffic study was completed as a part of the Environmental Assessment Worksheet (EAW) review, and has been updated. Hennepin County and the Minneapolis Park and Recreation Board (MPRB) have also reviewed the proposal and have comments and requirements that will need to be met. Detailed recommendations were included in that report and are incorporated here by reference. Notably, an access easement is requested along the southwest corner of the property to accommodate eventual relocation of driveways away from the Lowry/Stinson intersection. It should be noted that city requirements regarding access will match the permits issued by other agencies, since the MPRB and Hennepin County have jurisdiction over these roads.

Parking: There are 693 total parking stalls proposed for this development. The table below summarizes the parking type, stalls and area associated with each segment of the proposed development. The parking proposal meets city code standards. An additional 37 "proof of parking" stalls are proposed, to serve as additional parking, if needed.

Site Element	Enclosed Parking Stalls	Surface Parking Stalls	Stalls per unit
Building A	92 (underground)	34	2.06
Building B	77 (underground)	34	0.87
Building C	238 (underground)	218	2.03
TOTAL	407 stalls	286 stalls	693 total

Landscaping: The plan provides a landscaping program exhibit as well and cross sections and the separation of development from existing structures. Some trees are proposed to remain, especially those along the property borders. No trees will be removed that exist on the Kenzington property or

Park Board property (there is currently a row of trees along the Kenzington property and street trees along Stinson Parkway. However, most trees are planned to be removed and replaced. A landscape buffer is proposed along the northern and eastern edge of the property of evergreen trees. Staff finds the landscaping plan satisfactory to meet buffer goals for those edges.

Streetscape/Urban Design: The site plan and preliminary plat shows street design, sidewalks and trails incorporated into the project design. Setbacks shown on the site plan have been increased along Stinson to a minimum of 90 feet, along the alley setbacks are a minimum of 66 feet and along Kenzington Terrace property, a minimum of 65 feet. The revised plan has building orientation much more inside the proposed development, with large setbacks from property boundaries to the north, east, and west. Staff is requesting an evaluation of shifting the development another 20 feet away from the existing Kenzington Terrace building since there is a development on the eastern side and not as close on the western side of the development.

Trails/Sidewalks: The preliminary plat shows 5-foot sidewalks integrated into the site design along all internal roads, as well as an 8-foot sidewalk trail running along the northern site perimeter. While there is an existing sidewalk along both Stinson Blvd and Kenzie Terrace integrated into the project design, there are no proposed enhancements to existing sidewalks. Staff is recommending a 6 foot sidewalk instead of a 5 foot sitewide. Coordination with the Park Board to improve tripping hazards and ADA issues along Stinson Parkway will be required, especially at the intersection of the proposed private street.

Public Spaces/Parks: The preliminary site plan includes several private recreational amenities, including an outdoor pool, patio, two plazas, a small dog run. The proposal also includes several indoor private spaces. While all trails are proposed to be privately maintained, they will be open to the public to use.

Environmental Assessment Worksheet: A full report was completed for the analysis of the environmental impacts, including infrastructure impacts, contamination, traffic, and permitting required. The traffic report has been updated, per the revised site plan. Detailed information was analyzed, and this report is available on the city's website, or at city hall.

Environmental (Contamination): There is contamination on the site from the apparent spreading of oil on dirt roads for dust control (historically), the location of a dry cleaner on the Bremer site, and the siting of underground petroleum tanks (historical). The appropriate Response Action Plan has been submitted and approved by appropriate agencies. The developer is intending to remediate all environmental contamination on the site as a part of redevelopment. The EAW completed for this site has listed the detailed permits and information related to this clean up effort.

Grading: Significant grading is required on this property for the accommodation of the development proposal. The city engineer has reviewed the grading plan, and has submitted detailed comments in their memo.

Stormwater Management: A stormwater management collection and treatment system is proposed through a system of curbing, catch basins, underground conveyance, ponding, and bio-retention. This

includes traditional at grade ponds and below grade bio-filtration systems. Detailed comments have been submitted with the City Engineer's memo.

Utilities: Public sanitary sewer and watermain extensions and hydrants are proposed as part of this development. The infrastructure is proposed under private streets, with public easements over them. Detailed comments on the utilities have been submitted with the City Engineer's memo.

Phasing: After the final PUD approval, work will commence on the assisted senior living building as soon as weather conditions will allow. Construction is expected to take approximately 18 months to complete per building. As crews finish their particular subset of work, they will move to the independent senior living building and then to the market rate building. The Village is hopeful to commence work at the end of 2018 with an aggressive construction schedule to complete all three buildings by late 2020 or 2021.

Park Dedication Fees: Park dedication is required on all new subdivisions in St. Anthony. The amount of land (or cash in-lieu) will be determined at the time of final plat. All park dedication requests will need to be reviewed by the Park Commission (March 5th). More details will be needed to determine appropriate park dedication obligation and credit.

Development Agreements/Contracts: A series of development agreements and contracts will be required for this project. These legal documents will be drafted and prepared at appropriate times during the final plat/final development plan stage.

Other Agency/Department Requirements: In addition to city code requirements for the review and consideration of a Planned Unit Development preliminary development plan, re-zoning and subdivision application, this re-development is subject to many other jurisdictional requirements including the following:

State Rules Governing Completion of an Environmental Assessment Worksheet (EAW). Due to its size (over 375 attached units), State Rule 4410 requires preparation of an EAW to evaluate and identify existing environmental conditions and identify the impacts of this proposed development with regard to land use, soils, fish/wildlife, historical property, water resources, sewer, water, and transportation infrastructure and any potential noise, odor, light, or visual impacts. This document has been completed and several recommendations listed in this staff report are a result of this study.

Minnesota Pollution Control Agency. As part of the developer's purchase of the property, Phase I and Phase II Environment Assessments were completed and identified several sources of contamination on the property, which will need to be properly handled and remediated through the process established by the Minnesota Pollution Control Agency. Additionally, sanitary extension and construction permits will also be needed. Copies of all required permits will be required to be submitted prior to final approval of the development.

Minneapolis Park and Recreation Board. MPRB has submitted comments regarding the proposed development, outlining the process and permits required. The land under which Stinson Parkway is built is considered MPRB parkland, not right-of-way. Therefore, special review and provision must be made for any upgrades or impacts to Stinson Parkway, given its designation as parkland and the MPRB

jurisdiction. Copies of all required Park Board permits will be required to be submitted prior to final approval of the development.

Mississippi Watershed Management Organization. This development will be subject to the rules of the MWMO and the city regarding pollutant loading reduction, discharge rates and volumes, and flood elevations. The city is the permitting agency for the MWMO.

Fire Department. The St. Anthony Fire Department reviewed the proposal according to the 2015 Minnesota State Fire Code (MSFC), based on the 2012 International Fire Code. They noted the following points of consideration:

- *Access Roads:* Appear to be in compliance at this time. Concerns in regard to the bump outs prior to turning into the parking lots as well as the surmountable curb in the center of the complex road and concerning its size. That appears too large and trucks may not be able to make the turn without potentially having to drive over it.
- *Hydrant spacing:* Appears compliant along with distances to Fire Department Connections.
- *Building entrances:* Indicated with Fire Department Connections appear compliant. Garage entrances are indicated.
- When fire protection systems are designed early input from the fire department will make the process much more efficient.
- Considerations for each of the buildings cannot be addressed until detailed plans are submitted for the actual buildings including fire protection systems. A plan review process will occur for all buildings and will reference the codes in force when the plans are submitted. As was discussed in previous meetings, staff would like to see individual floor sprinkler shutoff's as well as an addressable alarm system.
- Entry door added next to each underground garage door access with a key box installed as well. This will provide direct entry access to the garage for any type of car fire, carbon monoxide issue, trash fire, medical or other incident that takes place there.
- Verification on the height of the overhang at the entry of the buildings. Consideration to height of apparatus as well as ambulance need to be given.

Public Safety. The St. Anthony Police Department reviewed the proposal and noted the following points of consideration:

- The Village site project located at 2501 Lowry Avenue is forecast to be a max 430 unit site, or 28 units per acre. The project site design includes a single boulevard style roadway. The project includes two three story pitched roof buildings that will consist of independent senior living, assisted senior living and memory care units. The site also contains a four story market rate apartment building. The site contains a single boulevard style roadway with no connections to the alley.
- Originally, concerns were raised with regard to access to the development off of Stinson Boulevard and Kenzie Terrace, as well as egress from the development onto these streets. An exit/entry point off of Lowry Avenue would be very close to the intersection of Lowry Avenue and Stinson Boulevard. Vehicles leaving the development onto WB Lowry Avenue would have low risk for accident. Those leaving onto EB Lowry Avenue and Kenzie Terrace would present

additional risk for vehicle, bicycle and pedestrian traffic. Vehicle exit/entry off of Stinson Boulevard would present minimal risk for area vehicle, bicycle and pedestrian traffic provided entry/egress is limited to NB traffic only. If a crossover to SB Stinson Boulevard is constructed, the risk potential would increase. It remains to be seen what influence the Minneapolis Park Board will have in regards to traffic management on Stinson Boulevard.

- The layout of outdoor parking at this development should be consistent with other area developments. It is recommended that the development allow for easy access for police patrol cars, fire apparatus and local ambulance services. Provisions for emergency vehicle access should allow for easy access/egress to the property. There should be continuity with regard to pedestrian and vehicle circulation within the site, which will aid in police, fire and EMS response. Turnarounds large enough to accommodate emergency vehicles and fire trucks should be considered. Building entrances should be in close proximity to pedestrian walk ways and vehicle parking areas.
- On street neighborhood parking on Stinson Boulevard and surrounding side streets, in St. Anthony and Minneapolis, may be impacted by a development of this size. Care should be given to future roadway design of Stinson Boulevard and Lowry Avenue/Kenzie Terrace. On street parking on Lowry Avenue should be avoided due to roadway design in that area. Due care should be taken in regard to a development of this size and existing residents as it pertains to quality of neighborhood life concerns related to traffic, parking, noise and littering complaints.
- The demands a 430-unit site puts on the police department can be projected in two ways. First, a performance based approach to staffing that takes into account projected call load. Second, and more common, is the average ratio of full time officers per 1,000 residents. The first, being more accurate, would be appropriate to consider. Seeing how this development will be phased in over multiple years, it will be important to do yearly monitoring of calls for service to this site when making staffing decisions.

Hennepin County Transportation. Hennepin County reviewed the proposal, providing notes on the site plan. Detailed review and comments are included as an exhibit.

COMMUNITY INPUT TO DATE

There has been much interest in this proposal, starting almost two years ago and continuing to-date. Staff has received over 100 emails since the sketch plan phase. Additionally, staff has spoken with many residents and concerned neighbors and has summarized the concerns as follows:

- Concerns over the density and height of the proposal. The most common concern is the opinion that the comprehensive plan should not be amended to accommodate this development and that heights should be consistent with those around the site.
- Concerns over the loss of affordable housing. People have expressed the need to retain/replace affordable housing on the site.
- Concerns over traffic and safety. People have stated the increase in traffic on Stinson Parkway will reduce livability and increase congestion at key intersections.
- Concerns regarding views for existing Kenzington Terrace residents. People have expressed concern over the reduction in views from their units.
- Concerns over tree removal.

- Concerns over school capacity and other public services.
- Concerns over the handling and treatment of stormwater runoff.

The revised plans reduce the density of the project, which results in almost half the traffic and population within the new proposal, increase the setbacks from property boundaries, and remove access from the alley. The revised plans show significant landscape buffering and some tree preservation, where practical. Stormwater management is located primarily in surface level ponds, rather than underground storage.

STAFF RECOMMENDATION

Staff has completed a thorough review of the proposal and has the following comments and conditions for the Preliminary PUD Development Plan and Preliminary Plat:

- ◆ Setback from Kenzington Terrace Condos. Increase the setback from the property line by approximately 20 feet, shifting the development to the west.
- ◆ Vacation of Easement. There is currently a watermain easement through the site, which will need to be vacated through a separate action accompanying a final plat.
- ◆ Engineering Comments. The full City Engineer memo and stormwater management memo are available as attachments and all changes should be made, per the memo to the satisfaction of the city engineer.
- ◆ Agency Comments. Park Board and Hennepin County permits shall be obtained and permit requirements should be incorporated, as final approval by those agencies requires.
- ◆ Revised Traffic Study Recommendations. All traffic mitigation efforts listed in the revised traffic study shall be part of the conditions for approval, as permitted by other regulating agencies (Park Board and Hennepin County).
- ◆ Receipt of All Applicable Permits. All agency permits shall be obtained, and plans approved prior to construction on the site.

In consideration of the above items, staff recommends that the Planning Commission take the following actions:

- Staff recommends approval of the preliminary PUD plan and preliminary plat, contingent on the requirements and items listed in the staff report and its attachments.

SUGGESTED MOTION

The following actions are options for the Planning Commission to consider:

- 1) **Motion to recommend approval of the preliminary PUD development plan and preliminary plat, contingent upon the items listed in the staff report and its attachments;**

- 2) Motion to table action on the preliminary PUD development plan, and preliminary plat, based on the submittal of additional information, as enumerated in the staff report;
- 3) Motion to recommend denial of the preliminary PUD development plan, and preliminary plat, and direct staff to prepare a draft resolution declaring terms of the same.

ATTACHMENTS

- 1) Village LLC narrative
- 2) Graphic Site Plan
- 3) Landscape Boards
- 4) Village LLC Civil Plans
- 5) Traffic Study, updated
- 6) Village LLC Building Exhibits, with parking information
- 7) City Engineer Memo
- 8) Storm Water Management Memo
- 9) Hennepin County Transportation Comments
- 10) Formal letter from resident



January 26, 2018

Mayor Faust & Council Members
c/o Mark Casey
CITY OF SAINT ANTHONY VILLAGE
3301 Silver Lake Road
Saint Anthony Village, Minnesota 55418

VIA HAND DELIVERY

RE: Written Statement in Support of PUD Application

Dear Mayor Faust and Council Members:

Having reached what THE VILLAGE, LLC ("The Village") believes to be a shared vision for redevelopment, it now submits a Planned Unit Development ("PUD") Preliminary Development Plan application (and, together with its related applications and materials, the "Applications") for redevelopment ("Project") of 2501 Lowry Avenue NE, Saint Anthony, Minnesota (the "Property"). This letter is offered as written support for the Applications.

In preparing the Applications, The Village scrapped its earlier plans and presents this entirely new concept. Understanding that a planned unit development must meet both The Village's needs and the needs of the City of Saint Anthony Village (the "SAV"), The Village Project was rebuilt from square one to address the concerns raised in opposition to the original applications. We began at "square one" with the baseline understanding that density would be dramatically reduced. This required a completely different way of looking at potential designs.

What evolved was a simpler design and greatly-reduced infrastructure. The revised design began with a single boulevard-style roadway, akin to Stinson Parkway. From there, architects refined the design to increase total parking (including adding significantly greater surface parking), reduce building heights, eliminate connections to the alley, reduce retaining wall use, and create deeper setbacks. The Project now includes two three-story, pitched-roof buildings to the north of the boulevard, used for independent senior living, assisted senior living and memory care units. A four-story, market rate apartment building rests south of the boulevard.

We have detailed the current unit breakdown by building below. But because minor changes can occur as the final drawings are fully refined, The Village proposes a hard cap of 430 units on the Property, or 28 units per acre.



You will also note that we have sought to balance the need for green space and the need for adequate parking. The Village believes that between the underground parking, the surface parking, and the street parking on the internal street there is sufficient parking for all uses in the Property. But it also recognizes that the demographics of each development and the parking needs of that demographic can vary dramatically between projects. To ensure that it has capacity if the reality of the Project proves more surface parking is necessary, The Village has built a buffer known as "proof of parking." If The Village is correct and it has adequate parking, precious green space will be preserved. But in the event that more parking is needed, The Village has identified a location of 37 additional stalls that would be constructed upon triggering events set out in the master development agreement that will be entered into between the City and The Village as part of any PUD approval.

Status of Bremer Property

Before turning to the specifics of the Applications, a clarification is in order about the status of the Bremer Property. The Village had a fully executed purchase agreement allowing it to acquire the Bremer Property (the "**Bremer PA**"). The Bremer PA has expired.

Additionally, The Village had previously reached a contingent agreement to eventually transfer the Bremer Property (and a portion of the Property) to Aeon (the "**Aeon PA**"). But the contingencies in the Aeon PA were not met, and the parties have agreed to cancel the Aeon PA. The Village never intended to take ownership of the Bremer Property or close on the Bremer PA. Instead, The Village expected to locate affordable housing on the Bremer Property as part of a mixed-use building. Discussions continue with Bremer while Bremer examines development options for the site.

Within the Applications, The Village looks to include the Bremer Property in the redevelopment tax increment district to give the eventual developer all available flexibility.

The Bremer Property is not included in the preliminary plat application. The eventual Bremer developer will receive the redevelopment tax increment designation and will have full control over the PUD process for the Bremer Property directly with the City.

Project Summary

A summary of the key components of the Project on the Property follows.

Site Element	Housing Type	Total Units	Height	Ugd. Parking	Surface Parking
Building A	Senior Ind. housing	61	3 story peaked roof 31'5" to main eave	92	34
Building B	Senior housing	128	3 story peaked roof 31'5" to main eave	77	34
Building C	Market-rate	225	4 story flat roof 44' to fascia	238	218
Total Units Proposed for 15.37 acres: 414 housing units				407	286
2501 Lowry Site Density: 26.94 units per acre					

The Proposal is a PUD, and the city has broad approval leeway to encourage development creativity that adds vitality to the city. The project however does not need much help in meeting the city's already established multifamily guidelines. The Project's Lot Coverage is projected to be 51.5% (including proof of parking). However, if we exclude the boulevard, that number falls to 47.1%. It is typical to exclude the boulevard for lot coverage calculations because the boulevard and the north and south sidewalks serve the public and the housing community. The 51.5% ratio also includes the swimming pool, gathering plaza, independent living patio and assisted living patio, and memory garden. These new outdoor spaces are landscaped and provide outdoor amenities for residents of the campus adding tremendous quality of life. If the pool and gathering plaza are excluded from these calculations, coverage drops further than 47.1%, which is excellent for a property of this size.

The Project's building-to-land ratio is 20.1% (22.3% if private road outlet is excluded), with the zoning max at 35%, the Project is well below the maximum.

Compliance with Planning Principles

In developing the Project, The Village paid special attention to the City's five Planning Principles. We discuss each below.

- 1) **The provision of affordable housing. The city has a history of including, requiring, and financially supporting the development of**

affordable housing in the community, and this commitment will continue;

The Village remains committed to affordable housing's role in the Project. Of the apartment units, 79 meet the standard for affordable housing at 80% of the Hennepin County area median income ("AMI"), which means that 35% of the apartment units qualify as affordable housing under U.S. Housing and Urban Development department standards.

The Village also proposes that a third party develop the adjacent Bremer Property for affordable housing. The Village will not develop the Bremer Property. It will, however, help ensure the future ability to offer affordable housing by requesting inclusion in the redevelopment tax increment district (eligibility for which it would not enjoy on its own). The Bremer Property's inclusion in the redevelopment district will provide maximum flexibility to its eventual developer.

Lastly, after the City denied the original applications, The Village and the City met to give the City a voice for its vision and provide a meaningful feedback loop. During the meetings, the City pointed out that SAV's HRA currently owns the 0.9-acre former bowling site (the "**HRA Property**"). To generate even greater affordable housing as part of the development, the City identified the HRA Property as a potential additional location for an affordable housing developer to construct housing at an affordability level that the City and community desire and that is financially possible. The Village agrees that locating additional affordable housing on the HRA Property in conjunction with the Project is logical and The Village looks forward to further discussions with all stakeholders to make maximal use of the HRA Property as a potential location for additional affordable housing.

2) Compatibility with surrounding land uses. It is important that new development match existing surrounding land uses in scale, height, and use;

For many years, the City's Comp Plan has designated the Property for high-density residential redevelopment. The Project appropriately balances that designation and the existing neighboring uses.

As the Comp Plan correctly notes, "[b]ecause the City is fully developed, additional housing would have to occur through redevelopment... [and] the anticipated density for this higher density housing development is 25 to 40 units per acre." The Village has worked to design a project close to

the bottom of that range. The Project can now reasonably be constructed at 28 units per acre or less. Accordingly, The Village seeks approval for a project built at 28 units per acre, largely because in its experience, market dynamics and design considerations can lead to minor variations in unit mixes before the project's conclusion. Rather than returning with a proposed amendment after the public process is complete, The Village requests a small density cushion now. The final PUD submission will detail the precise configuration and unit mix.

Additionally, the Project was designed as high-density to fit the neighborhood. The design began with the boulevard-style street intended to compliment Stinson Parkway and dissuade users from viewing it as an option for "cut through."

Next, the Project recognizes neighboring uses: predominantly single family to the west and north, and high-density residential or commercial to the east and south. The resulting configuration sees two senior-living buildings located north of the project's boulevard, with considerable setbacks from both the northern and western property lines and a three-story, pitched-roof design to more closely resemble a single-family home.

The Village strategically located the market-rate building on the Property's southern portion near existing high-density uses. Because it is market-rate, it is designed to appeal to the "renters-by-choice" demographic. These users demand that their apartment look and feel drastically different than a senior building. The architecture has a more modern feel as a result, featuring a flat roof and loaded with amenities, including a gathering area, pool, and dog run. The height was *technically* reduced to four stories, but it is lower than the peak of a three-story pitched roof.

- 3) Provision of adequate public infrastructure. All new development must provide appropriate infrastructure to manage water, sewer, runoff, streets, and access, and all other infrastructure needed to adequately serve itself and protect the surrounding community.**

The existing site is surrounded by public infrastructure available to serve the proposed, scaled back, development plan proposed. While the following paragraphs provide an overview, a more detailed engineering summary has been provided to touch on specific items that will be reviewed.

As the proposed development relates to traffic and access, the internal roadway, vehicle and pedestrian access and ways of traverse have been designed for the proposed demand. The offsite transportation improvements are being re-analyzed to account for the updated unit number and uses, which will present recommendations for offsite improvements. The preliminary development plans have been reviewed with engineering and fire departments and their respective initial comments have been incorporated.

The sanitary sewer adjacent to the site, located in the alley to the north, was determined in the previous submittals to be adequate to serve the development and that remains the case for this reduced density. Similarly, the domestic water supply is available to the south and west of the property to serve the development. As was the case in the previous proposals, following the preliminary approval of the proposed plan, the City will verify further improvements needed offsite to ensure adequate fire flows as required by proposed building types.

Stormwater management has been an obstacle on the Property for many years. Nonetheless, the Project meets the City and MWMO requirements for rate control, water quality and freeboard. The proposed management system consists primarily of a collection system routing runoff to traditional surface ponds, and supplemental filtration, to treat and store the stormwater prior to discharging it at a rate no more than the current conditions discharge.

You can find additional detailed engineering information in the submittal package attached on all of these items.

- 4) Protection of the environment. All development must meet all Federal, State, watershed district, and local requirements for environmental protection. The city will ensure all pollution and contamination is properly remediated.**

The Project is significantly scaled back from the 823-unit project. Notably, the environmental assessment worksheet for that initial project concluded it was not likely to result in significant environmental impact. The Village agrees that the Project must comply with all Federal, State, watershed district, and local requirements for environmental protection regulations. We believe it does meet – and, in many instances, exceeds – those thresholds.

As has been noted elsewhere, environmental testing reveals that hazardous substances exist below ground on the Property. Specifically,

the adjacent property and Property's prior uses have contaminated the environment with fuel oil and gasoline, diesel range organics, various volatile organic compounds, asbestos, and tetrachloroethene. The Village has already fully remediated the asbestos contamination and, together with appropriate governmental authorities, will undergo further remediation as part of the redevelopment. The Property is already part of the Minnesota Pollution Control Agency ("MPCA") Voluntary Investigation and Cleanup ("VIC") program and has received MPCA approval for its Voluntary Response Action Plan ("VRAP") to fully remediate contamination as part of the development.

5) A fair and open public process. When a formal application is received for development, the city will provide outlets for community discourse and discussion on the proposal.

The Village has sought to ensure a fair and open public process. After developing the plans for the Project, The Village held open community meetings to discuss the design and receive feedback. It also agreed to engage smaller groups to address specific questions. It has met with the Minneapolis Park and Rec department, Hennepin County Public Works, the Fire Chief and Fire Marshall, and SAV's engineer, all to gather as much input as possible to ensure the Project is truly a shared vision. In addition, the Applications will face the planning commission's scrutiny and a full public hearing.

Additional Considerations

Staging & Process

The Village seeks approval for its preliminary PUD application on the entire Project to get building dimensions approved and detail a shared vision for development. On receipt of that approval, The Village will work with staff on the terms of the master development agreement that is part of all PUD approvals. Once there is preliminary agreement on those terms, The Village will promptly submit for final PUD approval on the Project.

The phasing plan for the Project is significantly more compact than the original plans. After the final PUD approval, work will commence on the assisted senior living building as soon as weather conditions will allow. Construction is expected to take approximately 18 months to complete per building. As crews finish their particular subset of work, they will move to the independent senior living building and then to the market rate building. The Village is hopeful to commence work at the end of 2018 with

an aggressive construction schedule to complete all three buildings by late 2020 or 2021.

Parking.

Parking will be well-managed across the Project.

Based on the experience of the selected senior living operator, the senior buildings are likely over parked. The independent living building provides 126 stalls for 61 units, even though it is unrealistic to expect that a 2 to 1 parking ratio is necessary to meet parking demand. For the assisted living (which includes a memory care component), the expectation is one parking stall for every 3 units plus 20 employee stalls. Despite that ratio, the Project provides 111 stalls for 128 assisted living units, an excess of 48 stalls above the highlight standard.

When it comes to the market rate buildings, similar projects have generally been constructed with a 1.5 to 1.75 unit to parking stall ratio. But The Village is very committed to ensuring it has adequate parking. The market-rate building has 225 units (a total of 285 bedrooms). There are 238 below ground parking spaces and 181 surface stalls (without the proof of parking) for a parking ratio of 1.86. Sitting above market-average parking ratio, The Village finds the proposed parking adequate. As discussed above, however, The Village has built in an additional buffer of 37 stalls, that would be constructed if existing stalls proved to be inadequate, which would than slightly exceed a 2.0 unit to parking stall ratio.

Open Space and Park Dedication

An important component of the Project is that design choices were made to preserve a natural buffer along Stinson Parkway and along the northern and eastern property lines. By doing so, The Village seeks to save as many of the landmark trees in those areas as possible (upon further inspection some may show to be less than healthy and others cannot be saved due to the grading demands). Currently, The Village estimates that 17 such trees will be saved. In addition, the landscaping plan calls for the addition of new trees to enhance screening and enjoyment that comes from preserving such natural buffers. These areas will add to the trail system throughout the Project to create an open, park feel.

Property Control.

The Village has previously provided the City a copy of the deed for the Property.



Summary

To be clear, The Village is committed to working cooperatively with the City and, through that cooperation, believes it has reached a shared vision for redevelopment. The Applications are the result of a long process of evolution and refinement that has integrated additional green space, incorporated comments and feedback from stakeholders, and produced several interim concept plans. The Village and the City must finalize a development option that the City supports, that advances the City's guidance from its Comp Plan, and that the market justifies. This Project compliments the City's Comprehensive Plan, conforms to the zoning ordinance's overall intent and purpose, meets the City's planning principles, and puts the Property to its highest and best use.

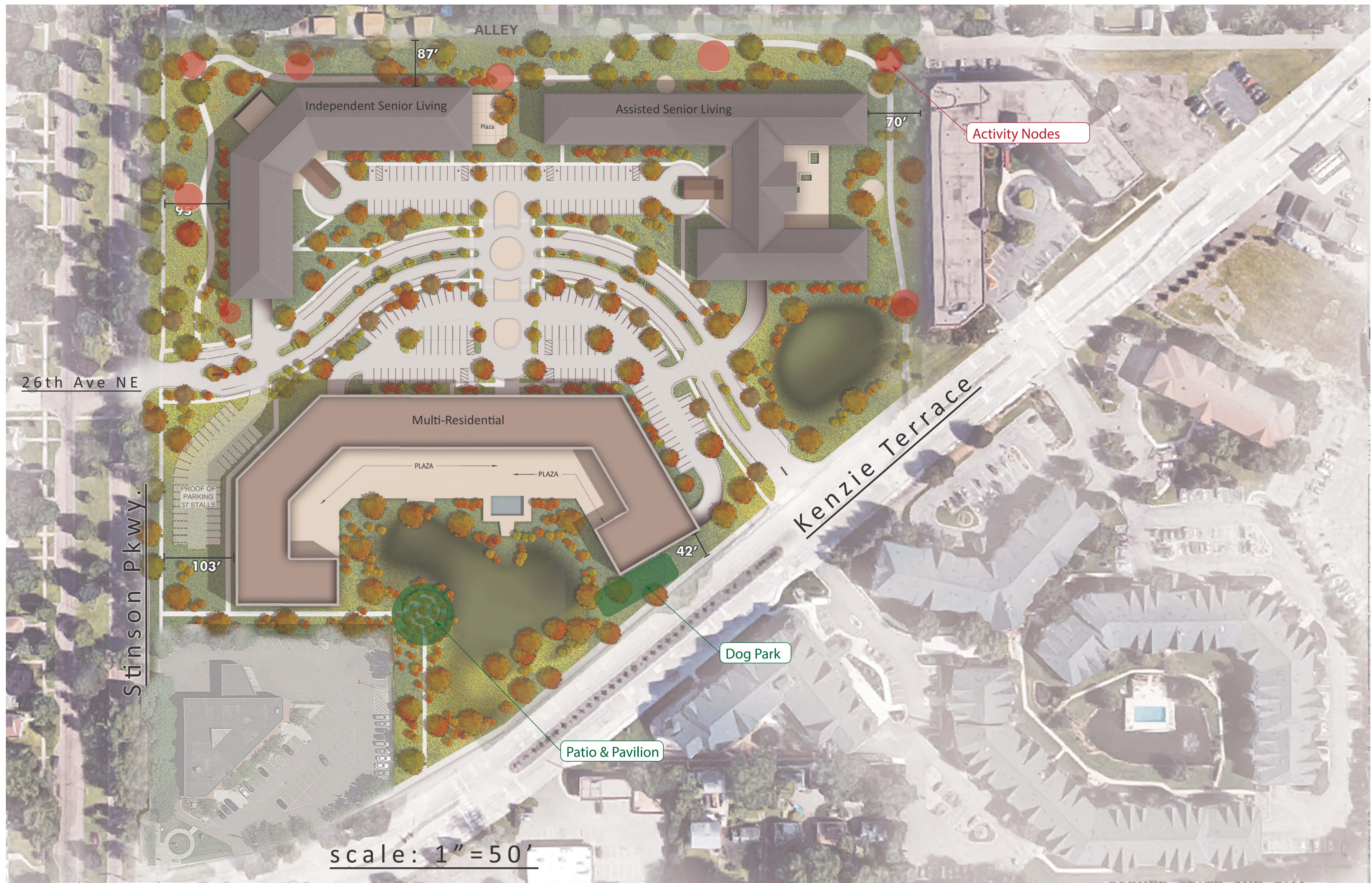
We look forward to discussing the application at the public hearing and, more importantly, working with the City to bring this ambitious project to a City and Property primed for redevelopment. Please do not hesitate to contact me with any questions you may have.

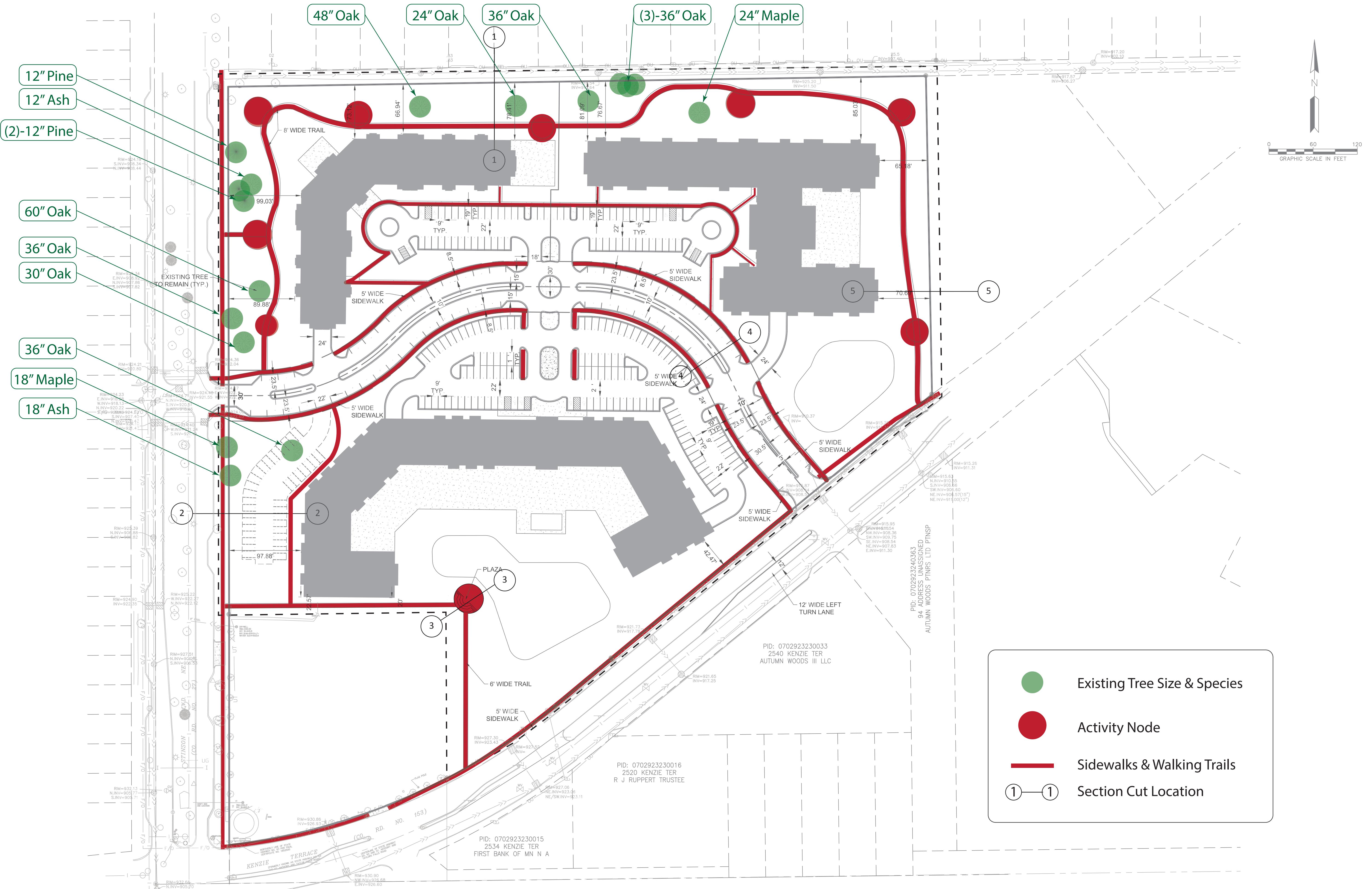
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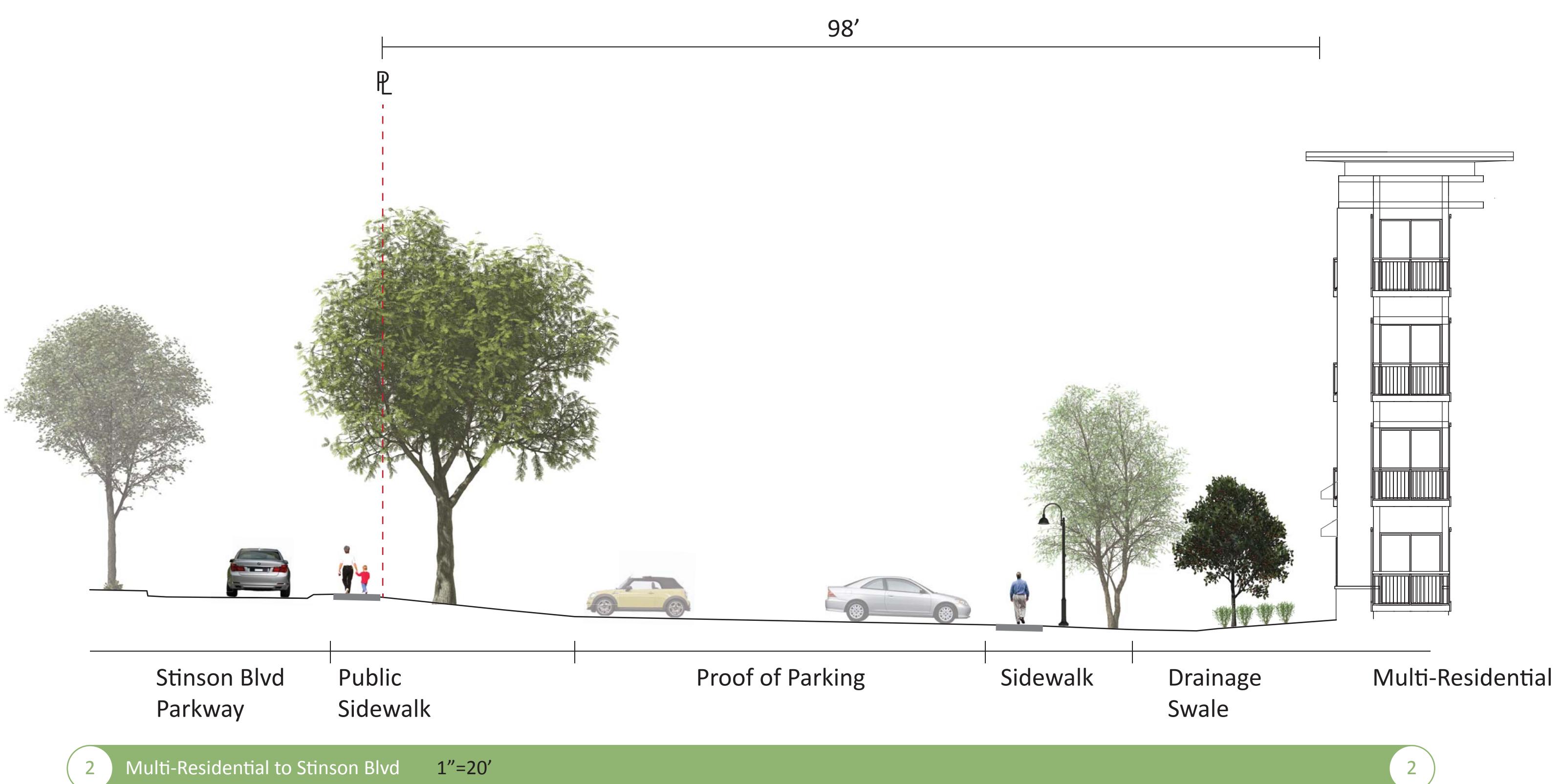
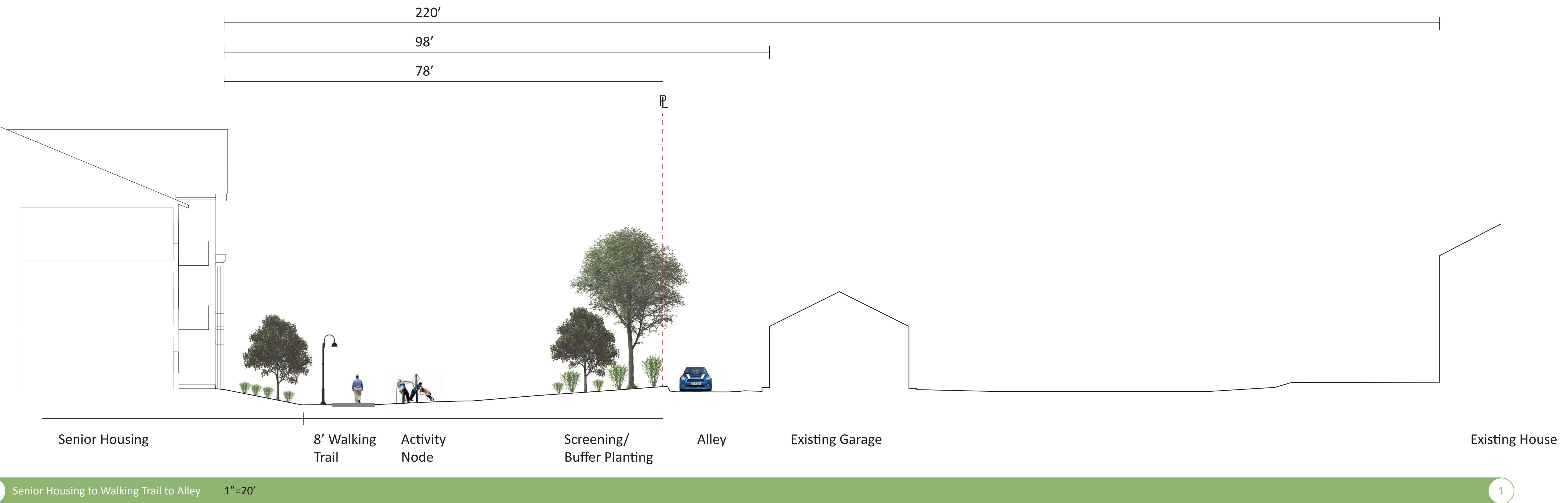
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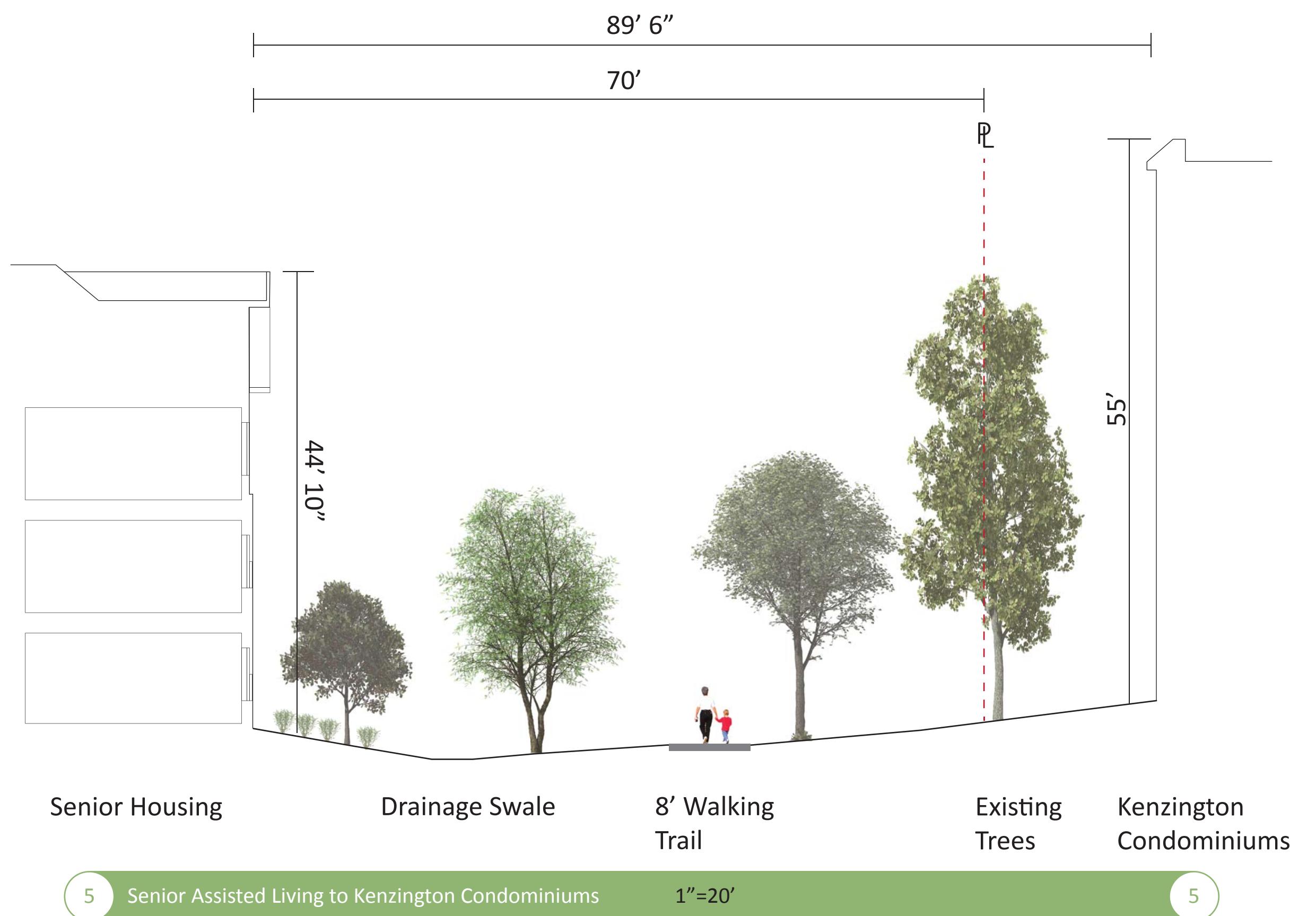
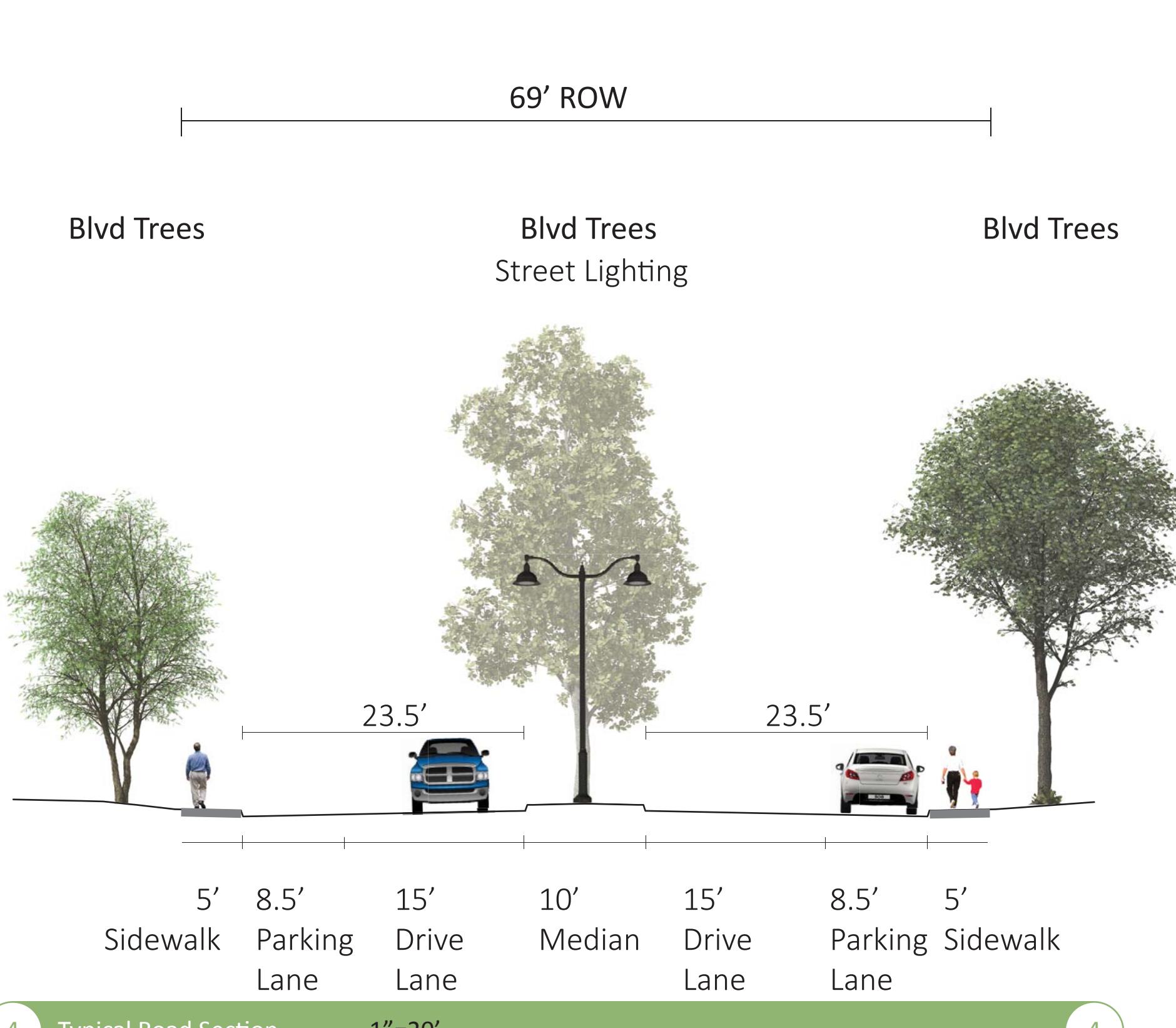
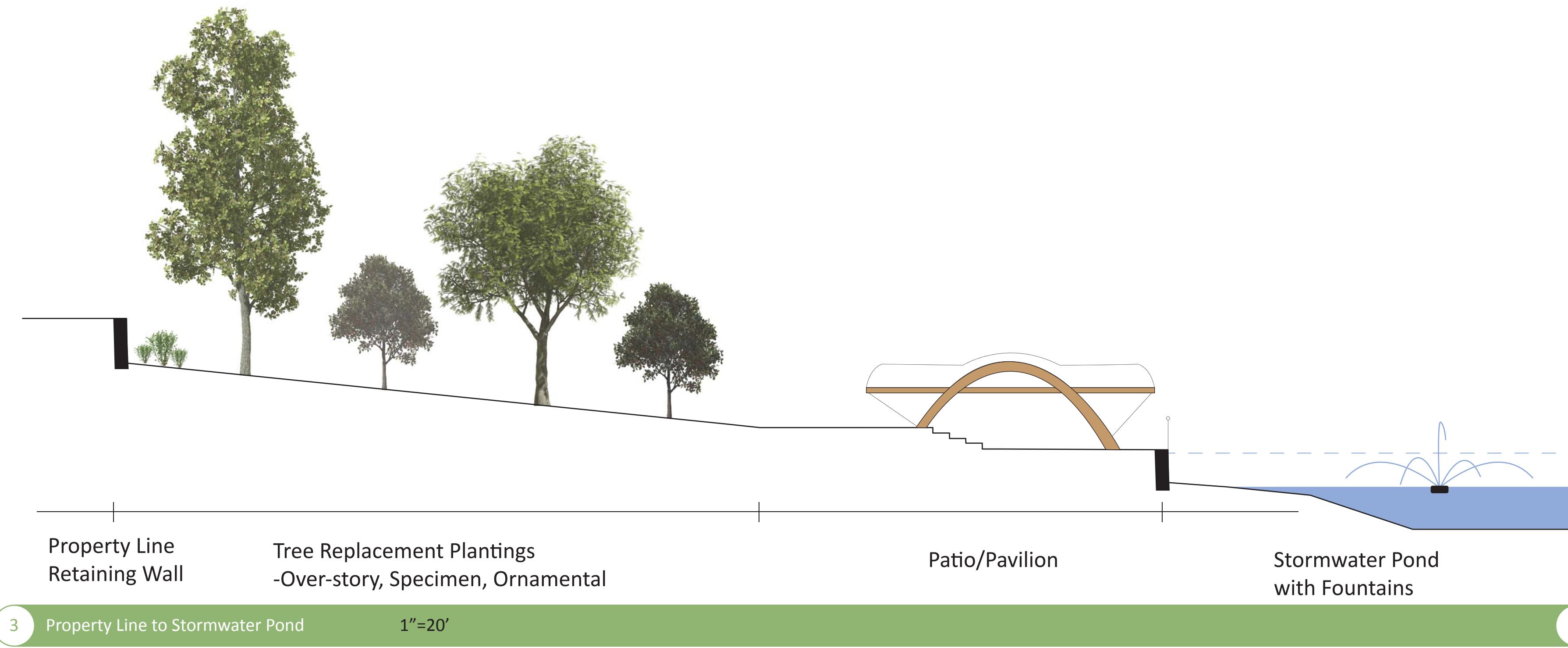
Traci Tomas
Vice President
The Village, LLC

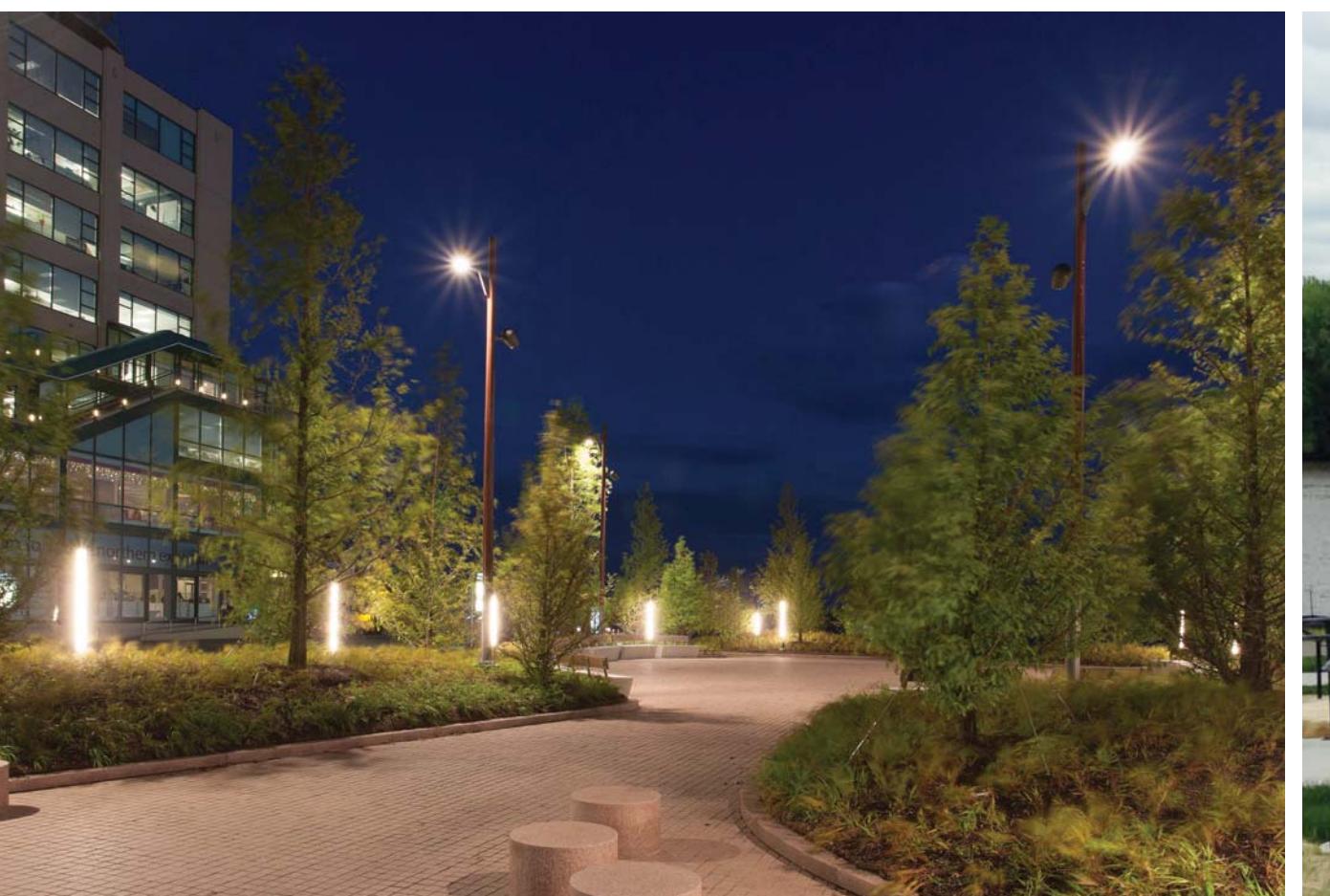




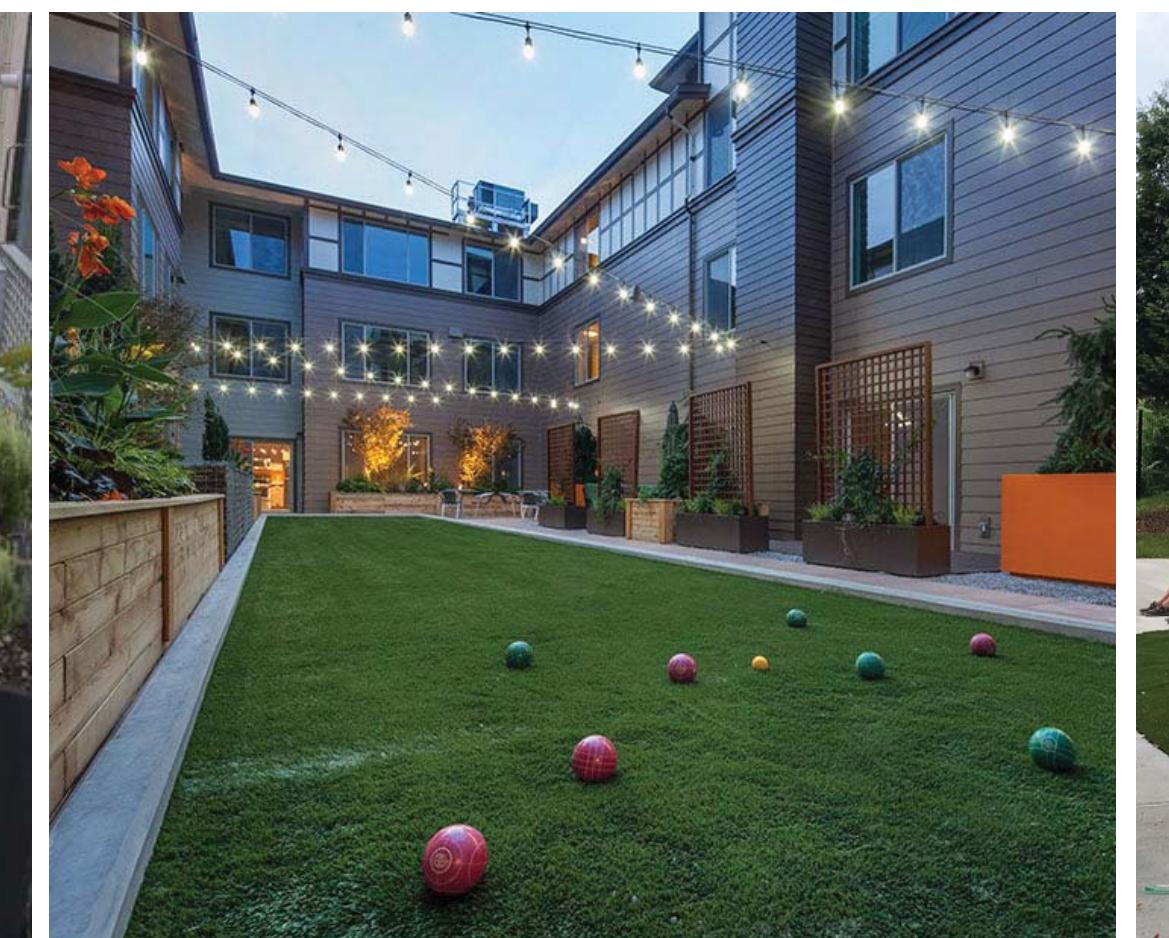




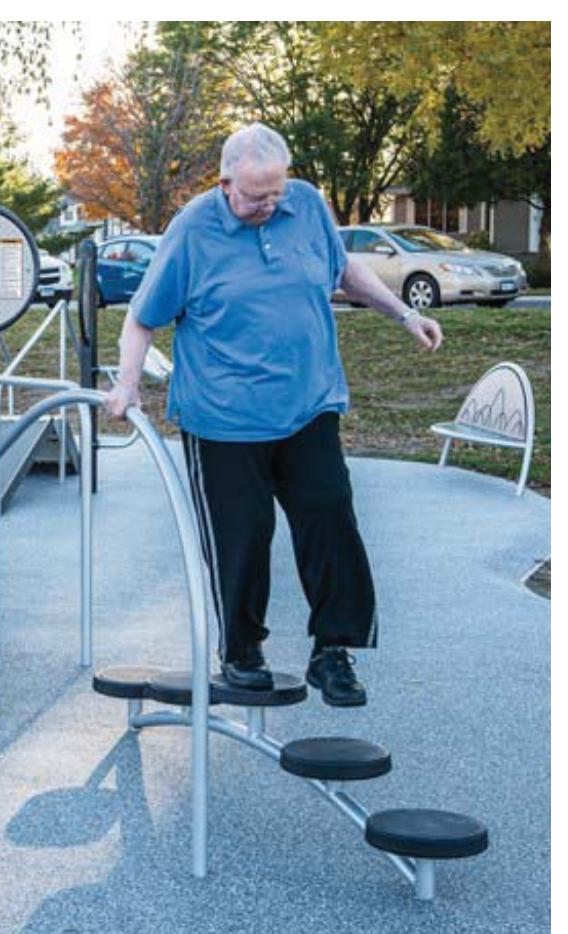




1 Village Park



2 Multi-Residential

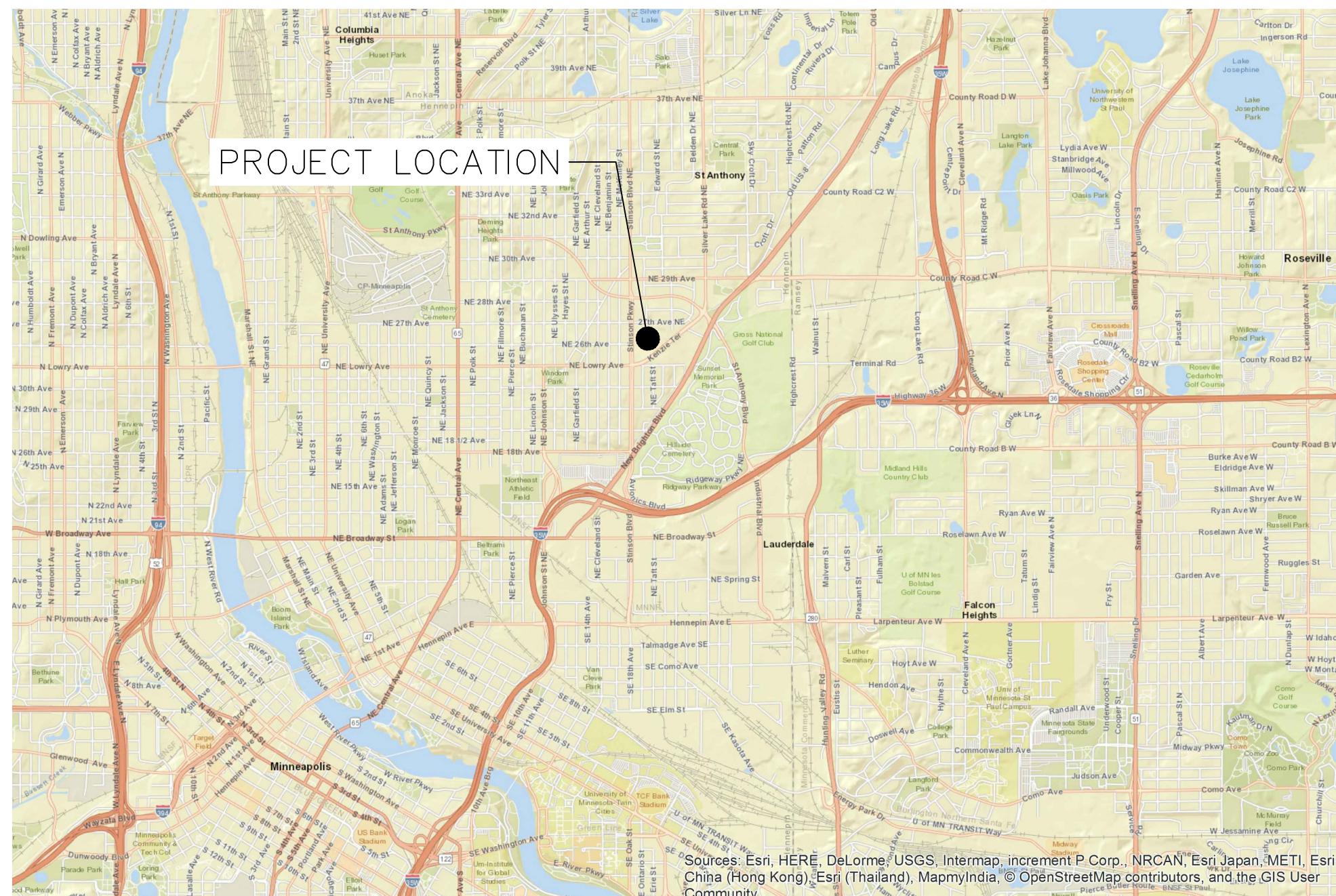


3 Senior Housing

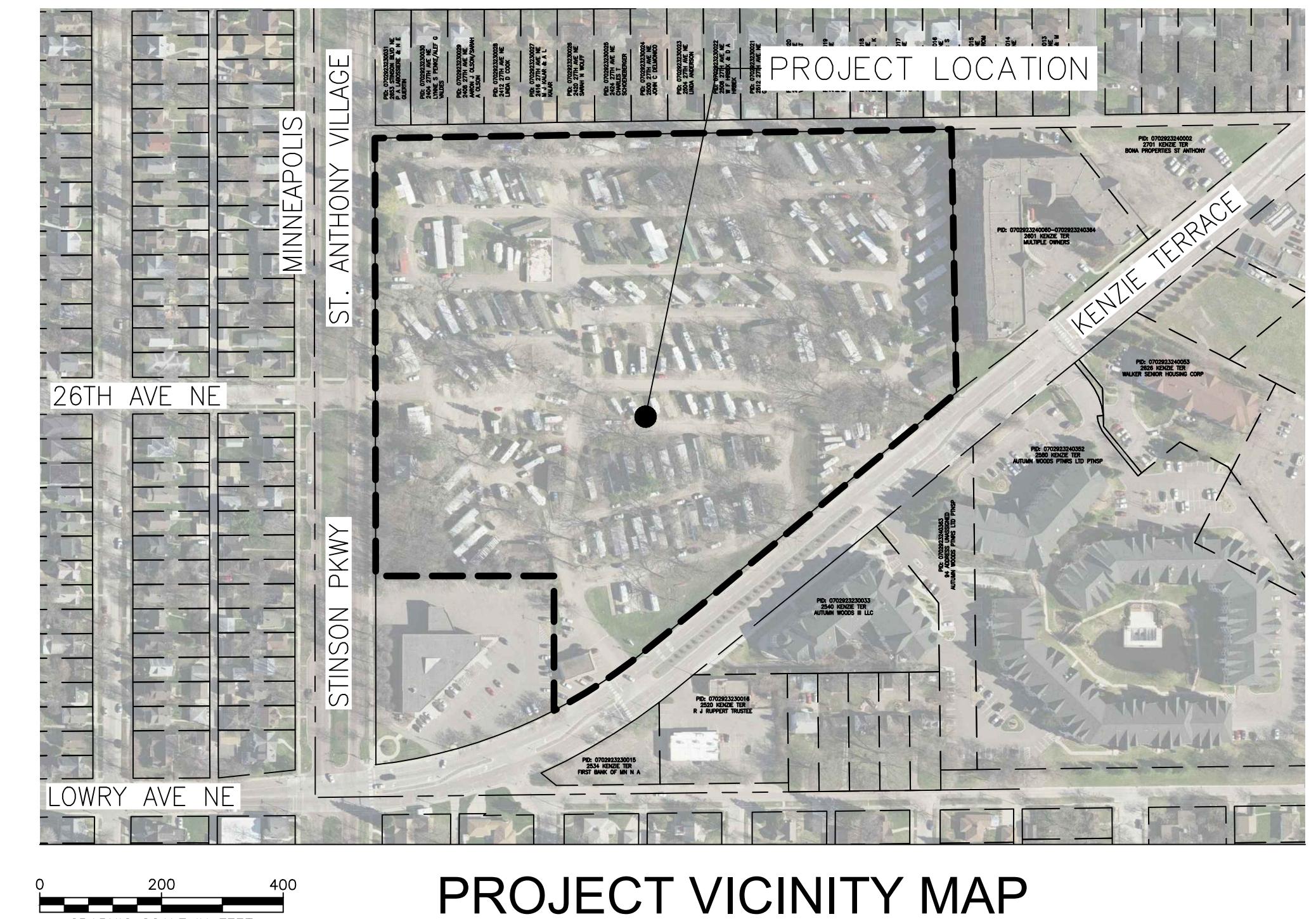
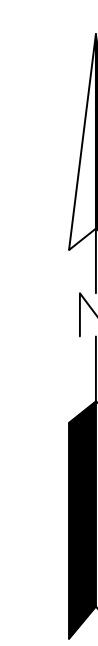
PRELIMINARY PUD PLANS FOR THE VILLAGE LLC DEVELOPMENT

CITY OF ST. ANTHONY VILLAGE
HENNEPIN COUNTY, MINNESOTA

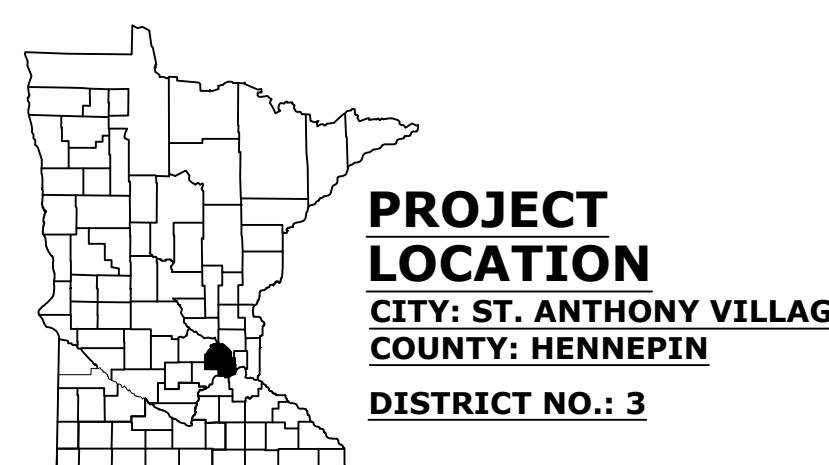
JANUARY 2018



PROJECT LOCATION MAP



PROJECT VICINITY MAP



GOVERNING SPECIFICATIONS:

1. CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD UTILITIES SPECIFICATIONS (LATEST EDITION)
2. THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" (LATEST EDITION).
3. ALL APPLICABLE FEDERAL, STATE, AND LOCAL LAWS AND ORDINANCES WILL BE COMPLETED WITHIN THE CONSTRUCTION OF THIS PROJECT.
4. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

INDEX OF SHEETS

	Issue #	Issue #
G-101	COVER AND INDEX SHEET	
G-102	NOTES AND LEGEND	
C-101	PRELIMINARY PLAT	
C-102	EXISTING CONDITIONS	
C-103	EXISTING CONDITIONS	
C-104	PRELIMINARY SITE PLAN	
C-105	PRELIMINARY SITE PLAN NORTH	
C-106	PRELIMINARY SITE PLAN SOUTH	
C-301	PRELIMINARY GRADING PLAN	
C-302	PRELIMINARY GRADING PLAN NORTH	
C-303	PRELIMINARY GRADING PLAN SOUTH	
C-401	PRELIMINARY UTILITY PLAN	
C-402	PRELIMINARY UTILITY PLAN NORTH	
C-403	PRELIMINARY UTILITY PLAN SOUTH	
C-501	PRELIMINARY STORM SEWER PLAN	
C-502	PRELIMINARY STORM SEWER PLAN NORTH	
C-503	PRELIMINARY STORM SEWER PLAN SOUTH	
C-801	DETAILS	
L-100	PLANTING PLAN - OVERALL CONCEPT	
L-101	PLANTING PLAN - CONCEPT SOUTH	
L-102	PLANTING PLAN - CONCEPT EAST	
L-103	PLANTING PLAN - CONCEPT WEST	

THIS SET CONTAINS 22 SHEETS

WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CALL BEFORE YOU DIG

GOPHER STATE ONE CALL

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166



Prepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

I HEREBY CERTIFY THAT THIS PLAN
SPECIFICATION, OR REPORT WAS
PREPARED BY ME OR UNDER MY
DIRECT SUPERVISION AND IS A
DULY REGISTERED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE
STATE OF MINNESOTA.

License #:
Date:

Project #:

Drawn By:

Issue Date:

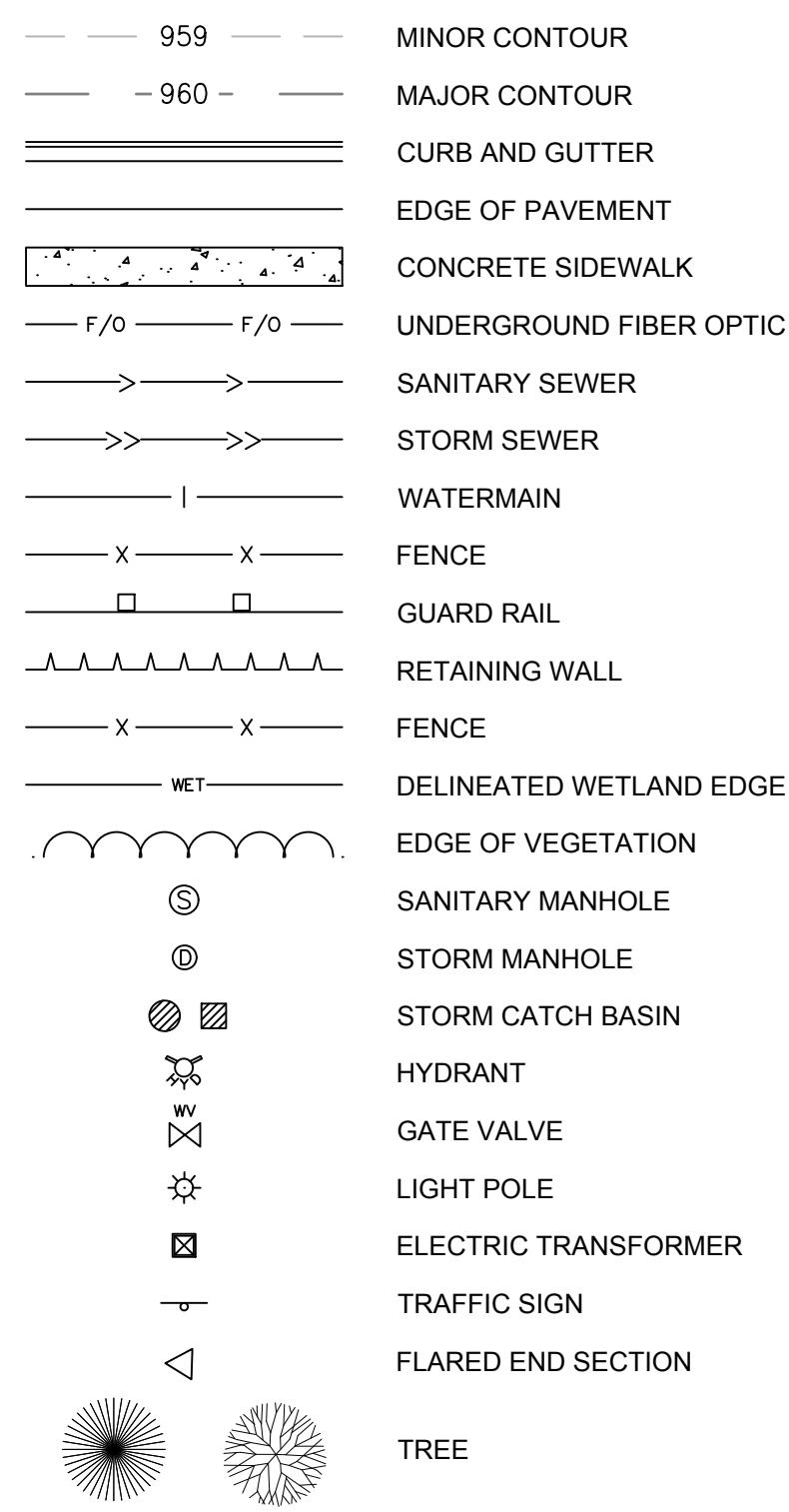
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Sheet #:

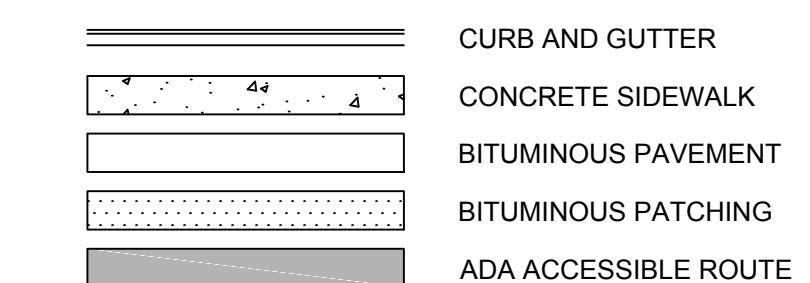
G-101

Sheet Title:
COVER AND
INDEX SHEET

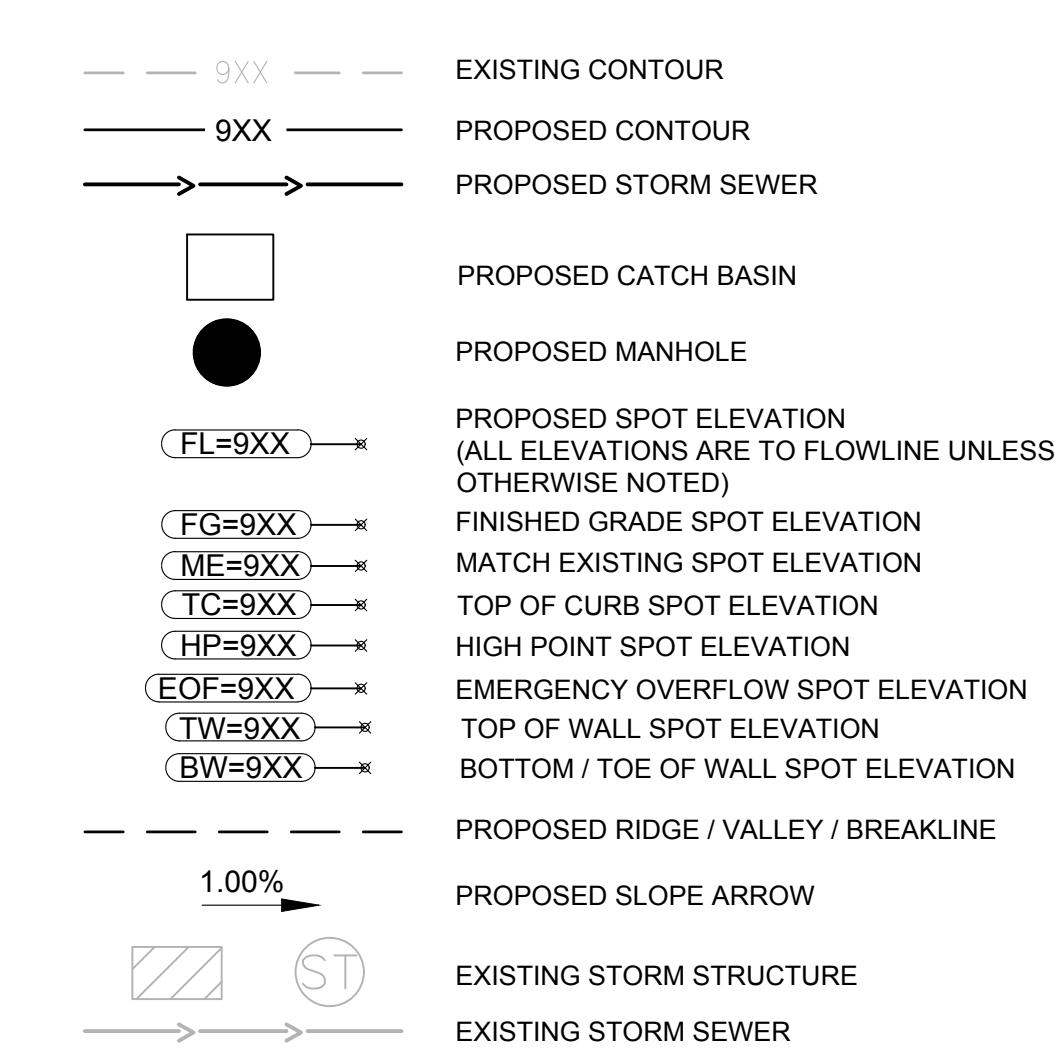
EXISTING LEGEND



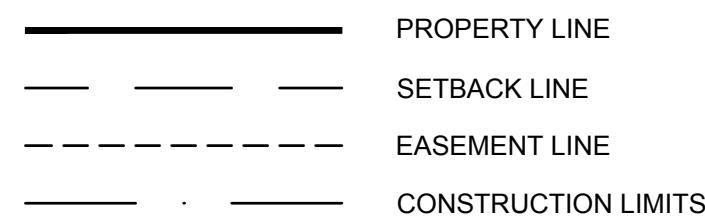
SITE PLAN LEGEND



GRADING LEGEND



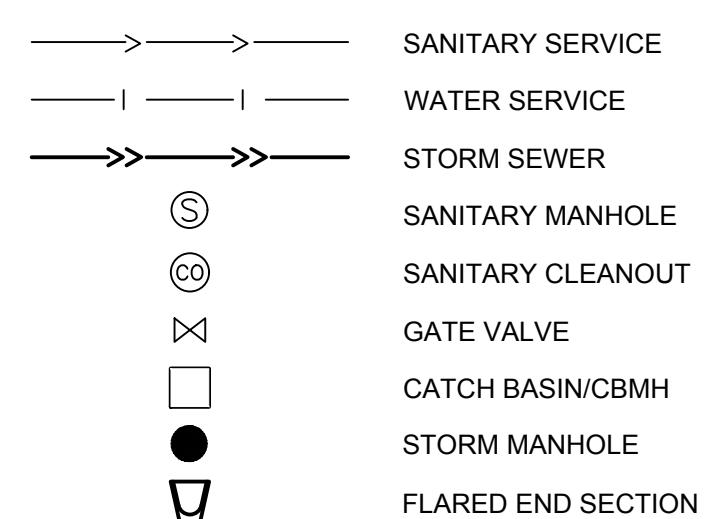
GENERAL LEGEND



REMOVALS LEGEND



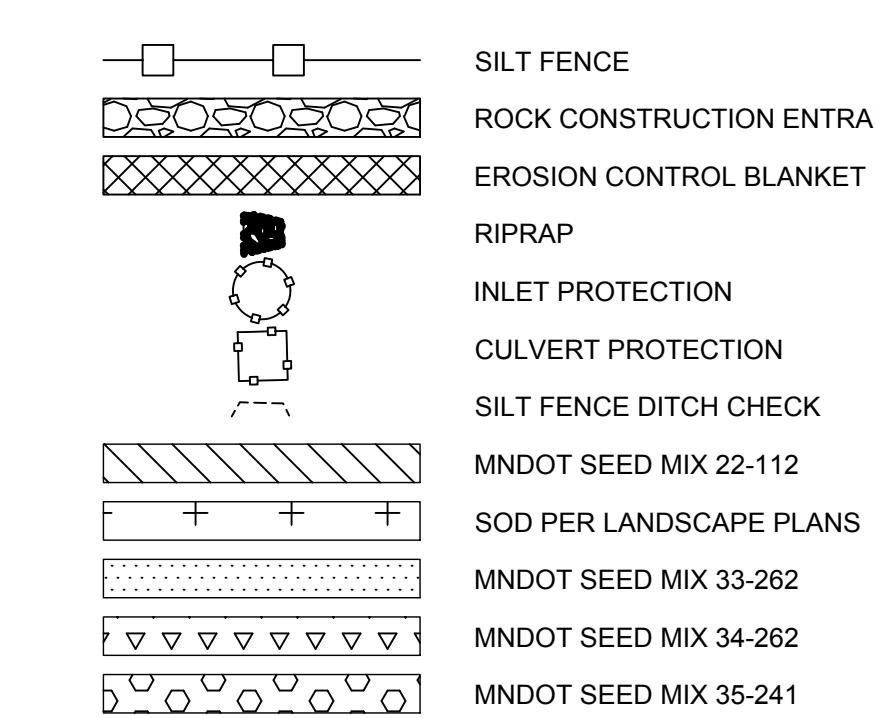
UTILITY/STORM SEWER LEGEND



GOVERNING SPECIFICATIONS:

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EROSION CONTROL LEGEND



WARNING:

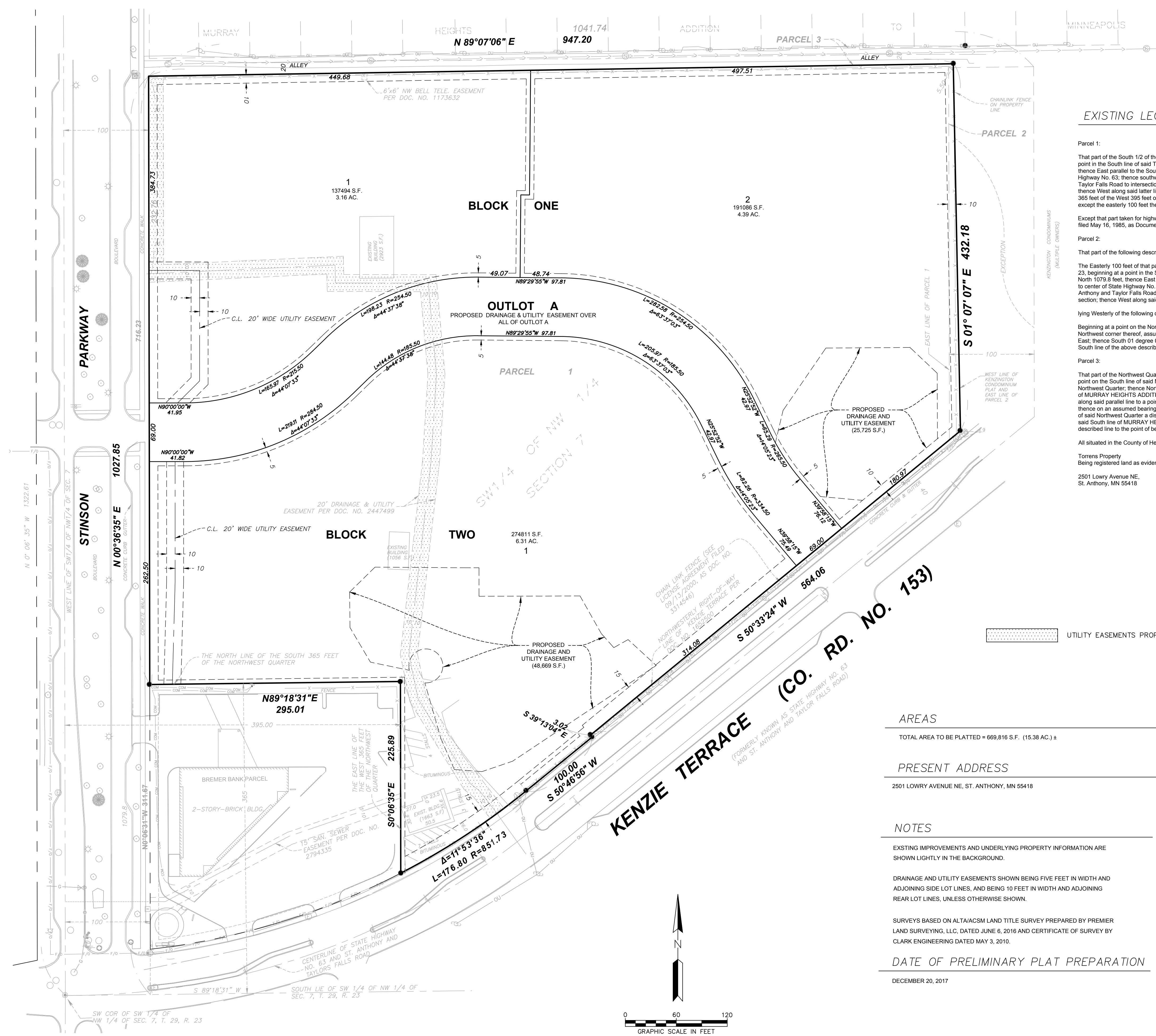
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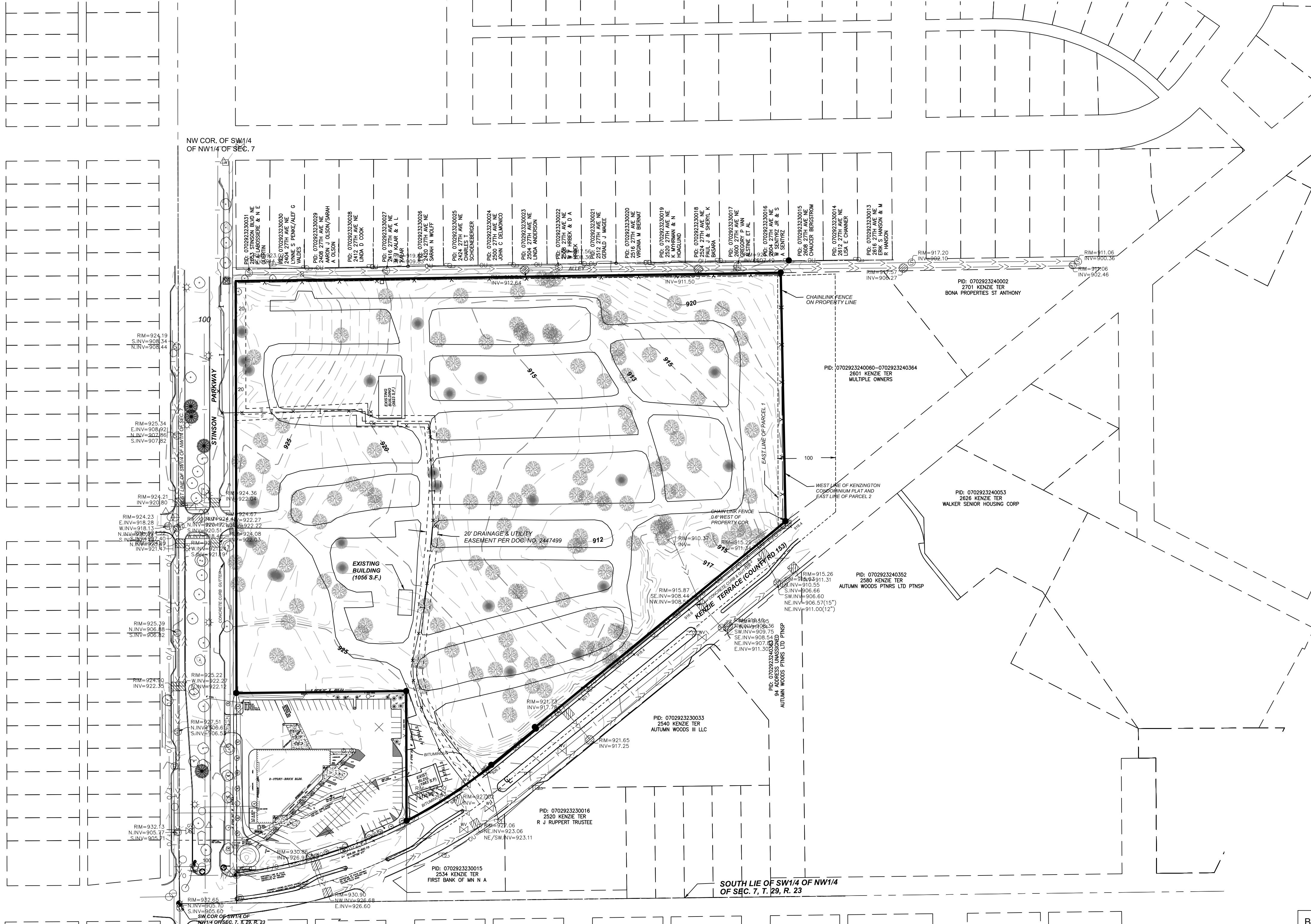
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CALL BEFORE YOU DIG

GOPHER STATE ONE CALL

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166





BENCHMARKS
 I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

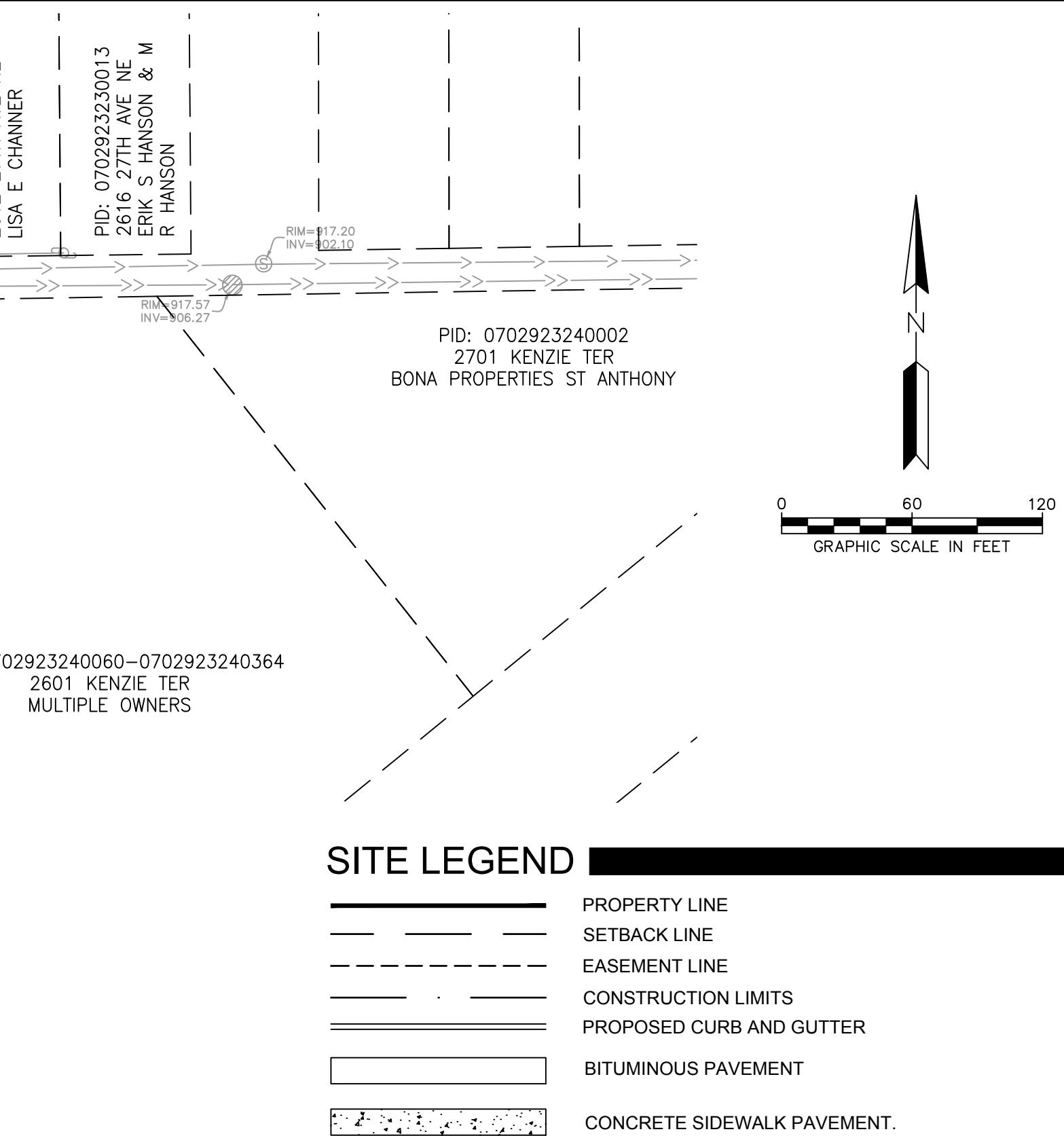
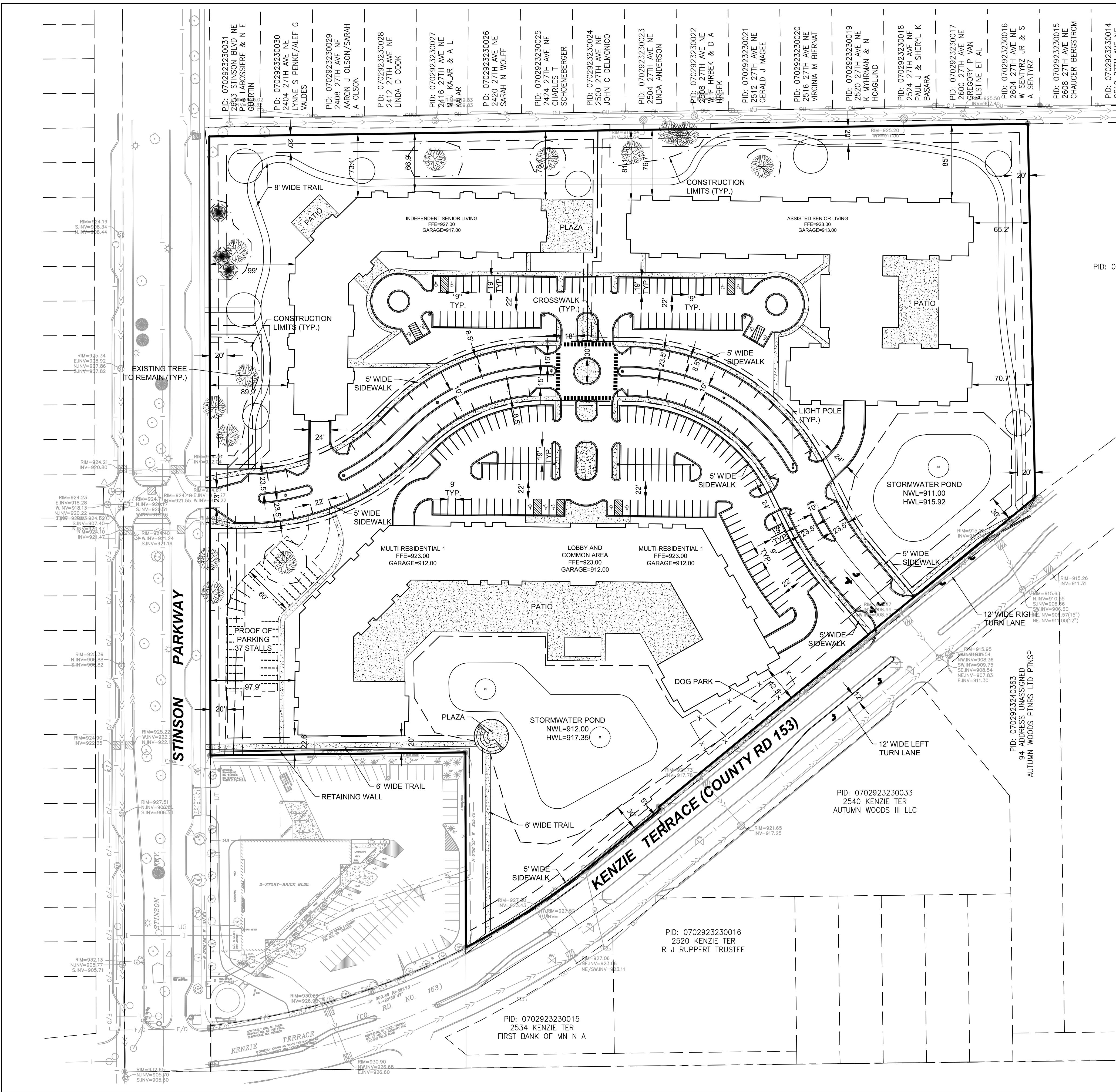
NOT FOR CONSTRUCTION
 Project #: 5005-0003
 Drawn By: RAG
 Issue Date: 12/06/2017
 Issue #: 1
 Sheet #: C-103
 Sheet Title: EXISTING CONDITIONS
 TWIN CITY AREA: 651-454-0002
 TOLL FREE 1-800-252-1166

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 CALL BEFORE YOU DIG
GOPHER STATE ONE CALL
 TWIN CITY AREA: 651-454-0002
 TOLL FREE 1-800-252-1166

SITE DATA	
PID: 0702923230001 (VILLAGE PROPERTY SITE)	
TOTAL AREA OF PROPERTY	669,885 SF (15.378 AC)
AREA DEVOTED TO	AREA SF (AC)
RESIDENTIAL-MANUFACTURED HOMES/RV'S	491,743 SF (11.289 AC)
COMMON SPACE	N/A
PUBLIC OPEN SPACE	N/A
STREETS	168,364 SF (3.865 AC)
OFF-STREET PARKING/LOADING (14 STALLS)	6,751 SF (0.155 AC)
AREA AND FLOOR AREA DEVOTED TO	AREA SF (AC)
COMMERCIAL USES	1,663 SF (0.038 AC)
INDUSTRIAL OR OFFICE	N/A

THE VILLAGE LLC DEVELOPMENT
ST. ANTHONY VILLAGE, MINNESOTA

Prepared For:
THE VILLAGE, LLC
 1907 WAYZATA BLVD, SUITE 250
 WAYZATA, MN 55391



SITE LEGEND

- PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- CONSTRUCTION LIMITS
- PROPOSED CURB AND GUTTER
- BITUMINOUS PAVEMENT
- CONCRETE SIDEWALK PAVEMENT.

SITE ANALYSIS TABLE

EXISTING ZONING	R1 SINGLE FAMILY RESIDENTIAL
PROPOSED ZONING	PUD - PLANNED UNIT DEVELOPMENT
LOT AREA	±669,585 SF (±15.4 AC)
TOTAL BUILDING AREA	±134,593 SF (20.1%)
SETBACK SUMMARY	BUILDING
EAST	20'
SOUTHEAST	30'
WEST	20'
NORTH	20'
SOUTHWEST	20'
INTERNAL	0'
GREENSPACE SUMMARY	PROPOSED
IMPERVIOUS AREA (INCL. BLDG AND POND)	±378,084 SF (8.68 AC)
PERVIOUS AREA	±291,501 SF (6.69 AC)
STANDARD	ADA
PARKING STALLS SUMMARY	RAMP SURFACE RAMP SURFACE TOT
BL. 1 LT. 1 - IND. LIVING	88 30 4 4 126
BL. 1 LT. 2 - AST. LIVING	86 30 4 4 124
BL. 2 LT. 1 - MULTI RES.	231 115 7 5 358
PARKWAY	0 61 0 0 61
ADDITIONAL PROOF STALLS	0 37 0 0 37
TOTAL PARKING COUNT	405 273 15 13 706

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

License #:

Date:

Project #:

Drawn By:

Issue Date:

Issue #:

Sheet #:

C-104

Sheet Title:

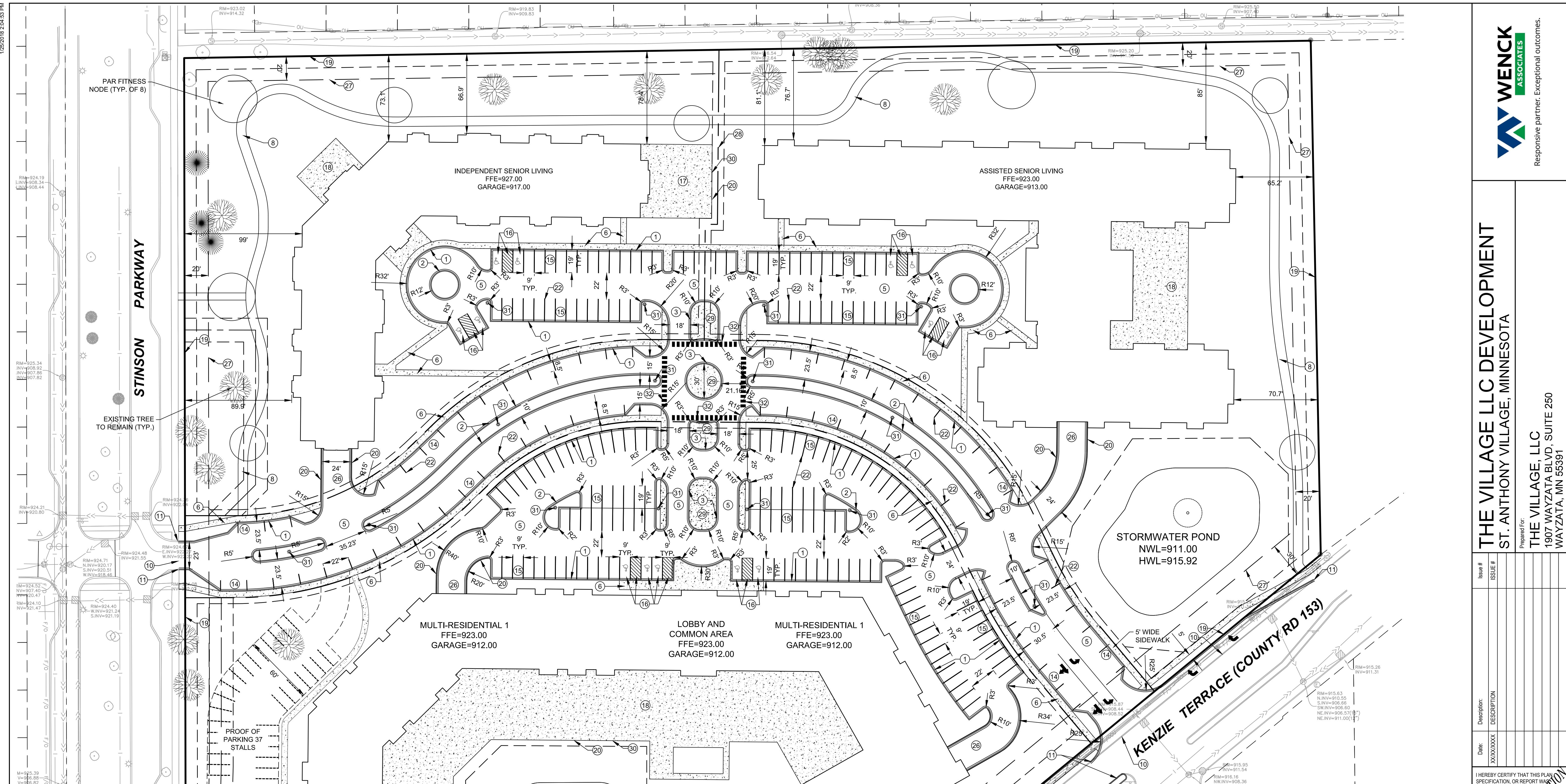
SITE PLAN

WARNING:
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CALL BEFORE YOU DIG

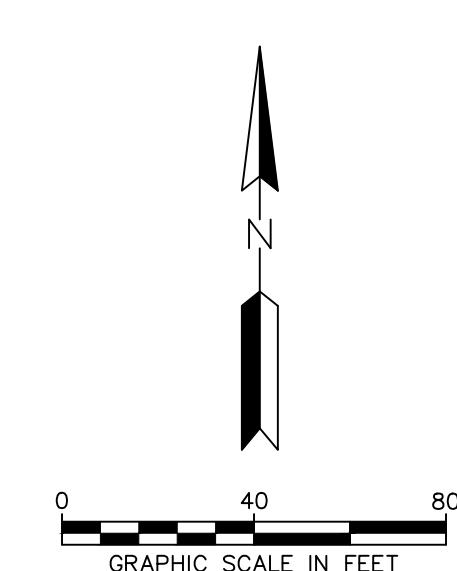
GOPHER STATE ONE CALL
TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166


SITE LEGEND

- PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- PROPOSED CURB AND GUTTER
- BITUMINOUS PAVEMENT
- CONCRETE SIDEWALK PAVEMENT

KEYNOTE LEGEND

- ① B612 CURB AND GUTTER
- ② B612 TIP-OUT CURB AND GUTTER
- ③ MOUNTABLE CURB AND GUTTER
- ④ 2' WIDE CONCRETE VALLEY GUTTER
- ⑤ BITUMINOUS PAVEMENT
- ⑥ 5' WIDE CONCRETE SIDEWALK
- ⑦ 6' WIDE CONCRETE TRAIL
- ⑧ 8' WIDE BITUMINOUS TRAIL
- ⑨ CONCRETE DRIVEWAY APRON
- ⑩ MATCH INTO EXISTING PAVEMENT
- ⑪ MATCH INTO EXISTING CURB AND GUTTER
- ⑫ MATCH INTO EXISTING SIDEWALK
- ⑬ PATCH BITUMINOUS PAVEMENT
- ⑭ PARALLEL PARKING STALLS - 22' X 8.5' TYP.
- ⑮ STANDARD PARKING STALL - 9' X 18' TYP.
- ⑯ ADA PARKING STALLS WITH ACCESS AISLE & CURB RAMP
- ⑰ PLAZA - SEE ARCH. AND LANDSCAPE PLANS
- ⑱ PATIO - SEE ARCH. AND LANDSCAPE PLANS
- ⑲ PROPERTY LINE
- ⑳ RETAINING WALL W/ RAILING. SEE ARCH. FOR RAILING
- ㉑ FENCE - SEE LANDSCAPING PLANS
- ㉒ 4" STRIPING
- ㉓ DOG PARK
- ㉔ TOT LOT/COMMUNITY AREA
- ㉕ ADA PEDESTRIAN RAMP
- ㉖ GARAGE ENTRANCE
- ㉗ BUILDING SETBACK
- ㉘ LOT LINE
- ㉙ DECORATIVE PAVEMENT SEE LANDSCAPE PLANS
- ㉚ RAILING - SEE ARCH. PLANS
- ㉛ STREET LIGHT
- ㉜ CROSSWALK


WARNING:

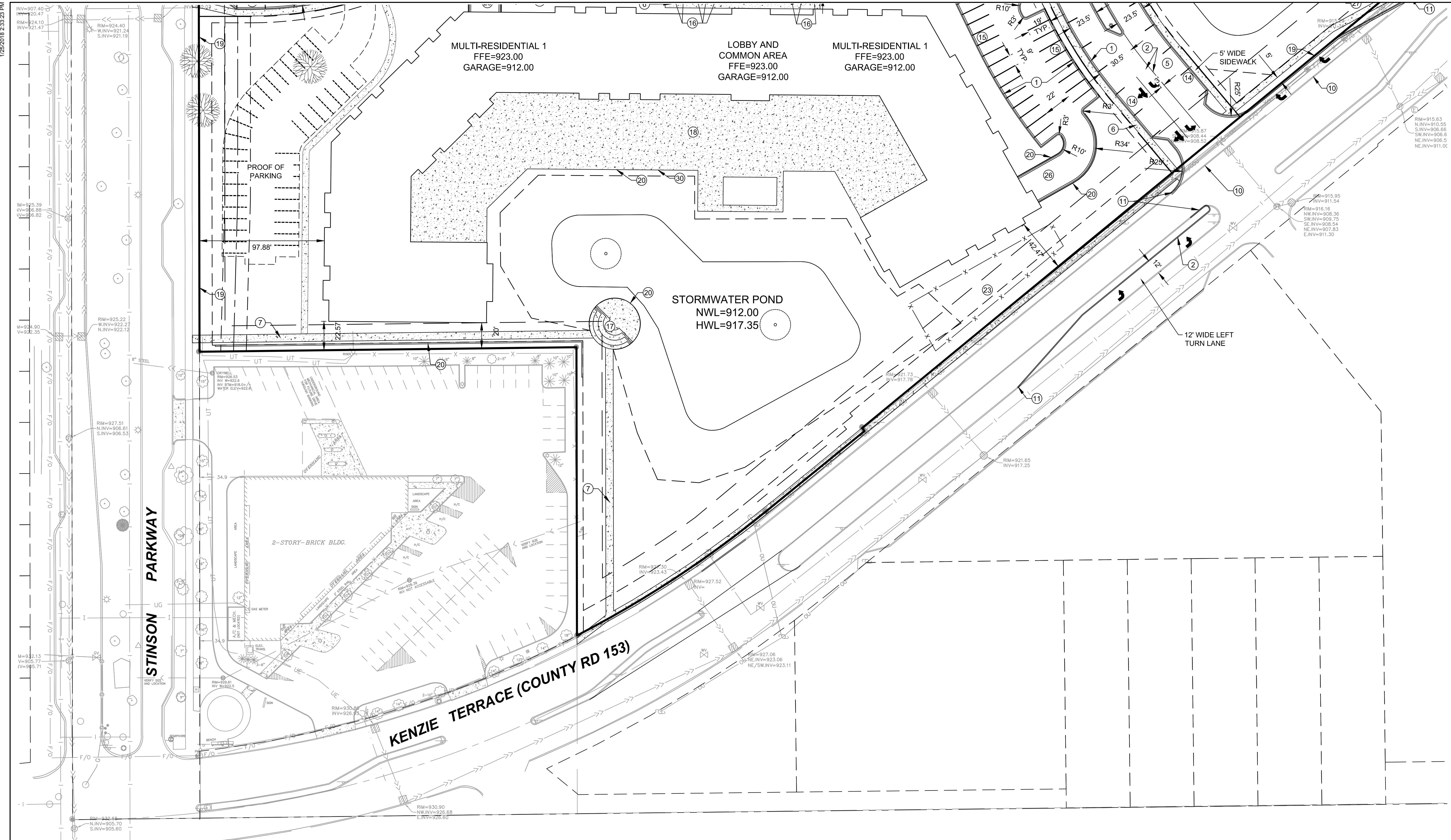
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CALL BEFORE YOU DIG

GOPHER STATE ONE CALL

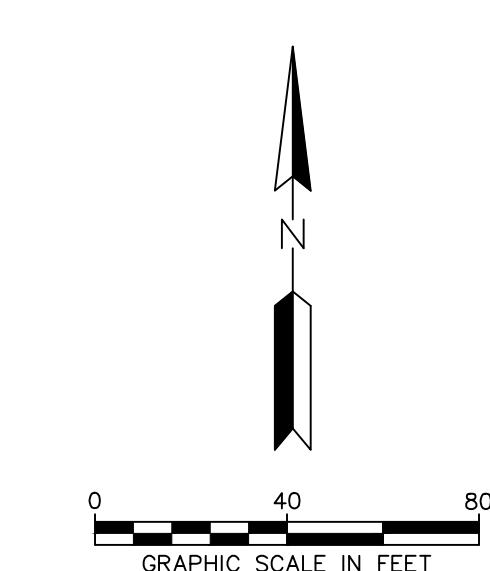
TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166

**SITE LEGEND**

- PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- PROPOSED CURB AND GUTTER
- BITUMINOUS PAVEMENT
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- ㉛ STREET LIGHT
- ㉜ CROSSWALK

**WARNING:**

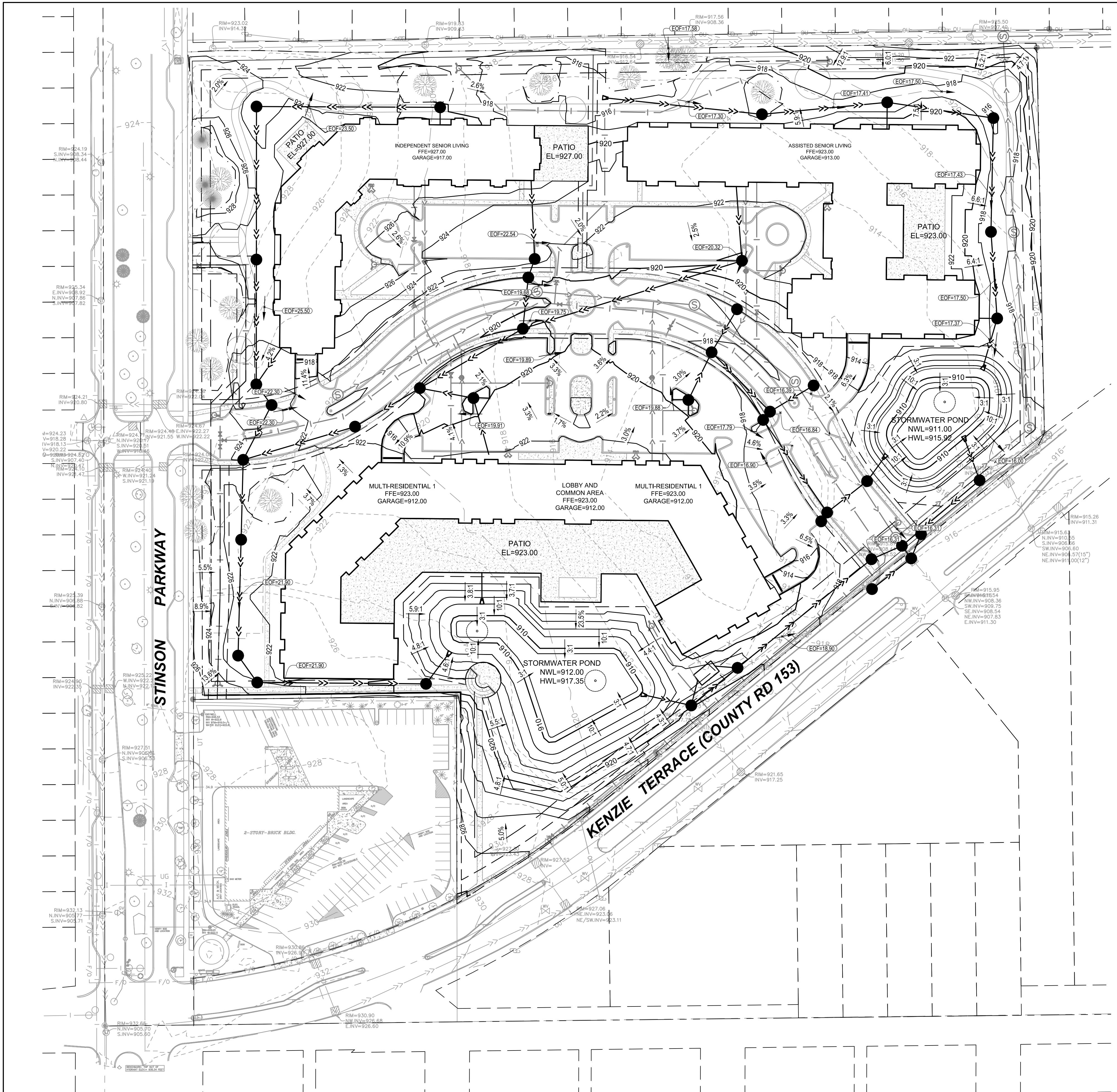
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CALL BEFORE YOU DIG

GOPHER STATE ONE CALL

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166



GRADING LEGEND

CONSTRUCTION LIMITS
PROPERTY LINE
SETBACK LINE
EASEMENT LINE
EXISTING CONTOUR
PROPOSED CONTOUR
PROPOSED STORM SEWER
PROPOSED CATCH BASIN
PROPOSED MANHOLE
PROPOSED SPOT ELEVATION (ALL ELEVATIONS ARE TO FLOWLINE UNLESS OTHERWISE NOTED)
FINISHED GRADE SPOT ELEVATION
MATCH EXISTING SPOT ELEVATION
TOP OF CURB SPOT ELEVATION
HIGH POINT SPOT ELEVATION
EMERGENCY OVERFLOW SPOT ELEVATION
TOP OF WALL SPOT ELEVATION
BOTTOM / TOE OF WALL SPOT ELEVATION
PROPOSED RIDGE / VALLEY / BREAKLINE
EOF FLOW ARROW
PROPOSED SLOPE ARROW
EXISTING STORM STRUCTURE
EXISTING STORM SEWER

NOTE: SEE C-501 STORM SEWER PLAN FOR STORM SEWER DESIGN DETAILS

THE VILLAGE LLC DEVELOPMENT
ST. ANTHONY VILLAGE, MINNESOTAPrepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

I HEREBY CERTIFY THAT THIS PLAN
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PREPARED BY ME OR UNDER
DIRECT SUPERVISION OF A
DULY REGISTERED PROFESSIONAL
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STATE OF MINNESOTA.

License #:
Date:

Project #:
5005-0003

Drawn By:
RAG

Issue Date:
12/06/2017

Issue #:
1

Sheet #:
C-301

Sheet Title:
GRADING
PLAN

WARNING:

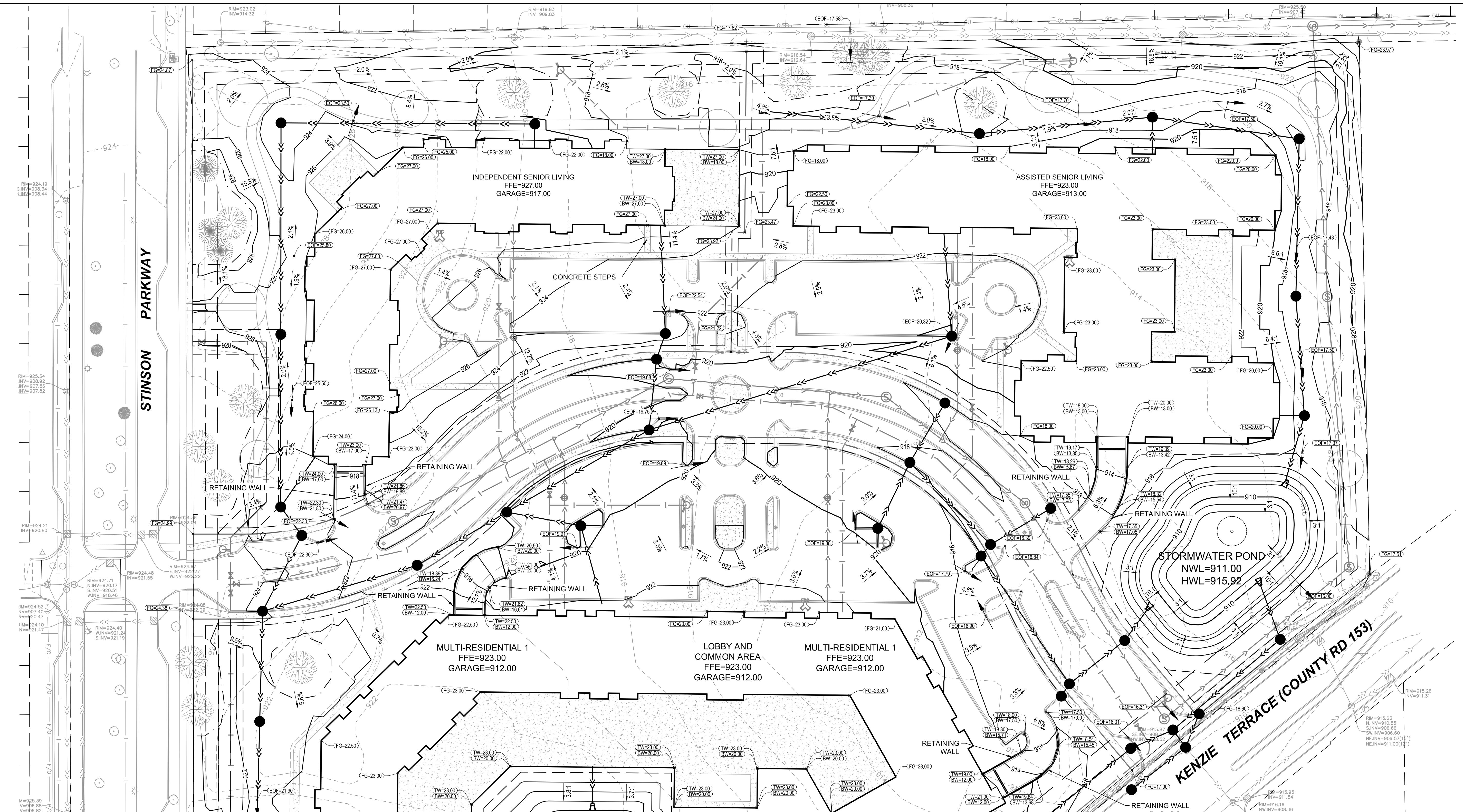
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TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166



GRADING LEGEND

- CONSTRUCTION LIMITS
- PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED STORM SEWER
- PROPOSED CATCH BASIN
- PROPOSED MANHOLE

NOTE: SEE C-501 STORM SEWER PLAN FOR STORM SEWER DESIGN DETAILS

- PROPOSED SPOT ELEVATION
(ALL ELEVATIONS ARE TO FLOWLINE UNLESS OTHERWISE NOTED)
- FINISHED GRADE SPOT ELEVATION
- MATCH EXISTING SPOT ELEVATION
- TOP OF CURB SPOT ELEVATION
- HIGH POINT SPOT ELEVATION
- EMERGENCY OVERFLOW SPOT ELEVATION
- TOP OF WALL SPOT ELEVATION
- BOTTOM / TOE OF WALL SPOT ELEVATION
- PROPOSED RIDGE / VALLEY / BREAKLINE
- EOF FLOW ARROW
- PROPOSED SLOPE ARROW
- EXISTING STORM STRUCTURE
- EXISTING STORM SEWER

0 60 120
GRAPHIC SCALE IN FEET

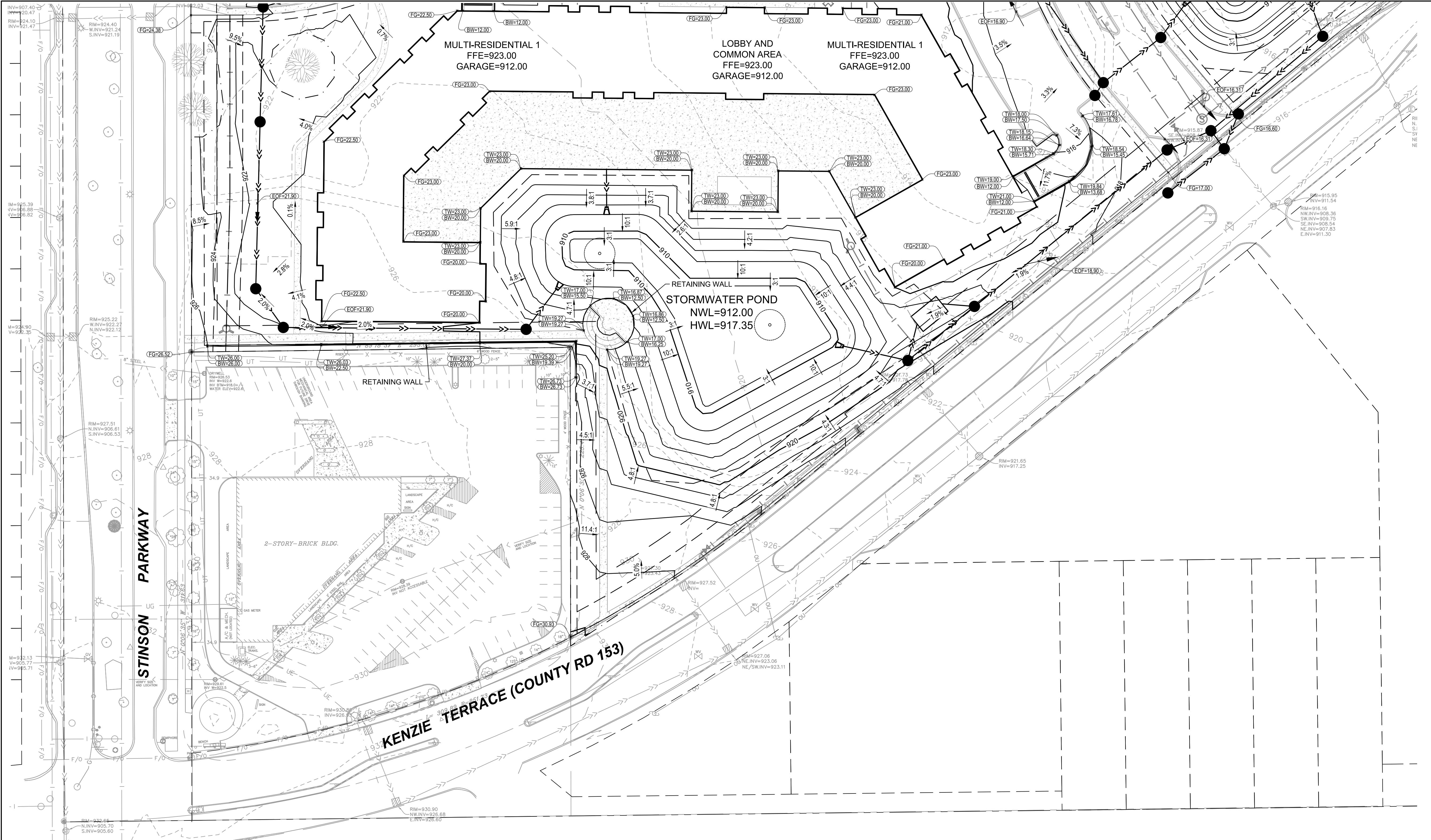
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CALL BEFORE YOU DIG

GOPHER STATE ONE CALL
TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166



GRADING LEGEND

	CONSTRUCTION LIMITS		PROPOSED SPOT ELEVATION (ALL ELEVATIONS ARE TO FLOWLINE UNLESS OTHERWISE NOTED)
	PROPERTY LINE		FINISHED GRADE SPOT ELEVATION
	SETBACK LINE		MATCH EXISTING SPOT ELEVATION
	EASEMENT LINE		TOP OF CURB SPOT ELEVATION
	EXISTING CONTOUR		HIGH POINT SPOT ELEVATION
	PROPOSED CONTOUR		EMERGENCY OVERFLOW SPOT ELEVATION
	PROPOSED STORM SEWER		TOP OF WALL SPOT ELEVATION
	PROPOSED CATCH BASIN		BOTTOM / TOE OF WALL SPOT ELEVATION
	PROPOSED MANHOLE		PROPOSED RIDGE / VALLEY / BREAKLINE
			EOF FLOW ARROW
			PROPOSED SLOPE ARROW
NOTE: SEE C-501 STORM SEWER PLAN FOR STORM SEWER DESIGN DETAILS			EXISTING STORM STRUCTURE
			EXISTING STORM SEWER

NOTE: SEE C-501 STORM SEWER PLAN
STORM SEWER DESIGN DETAILS

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CONSTRUCTION AT NO COST
CALL BEFORE YOU DIG

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GOPHER STATE ONE CALL

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166

REBLY CERTIFY THAT THIS PLAN,
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Y REGISTERED PROFESSIONAL
INEER UNDER THE LAWS OF THE
TE OF MINNESOTA.

NSA #:

Wor(l)d _____

5005 0000

ect #: 5005-0003

wn By: **RAG**

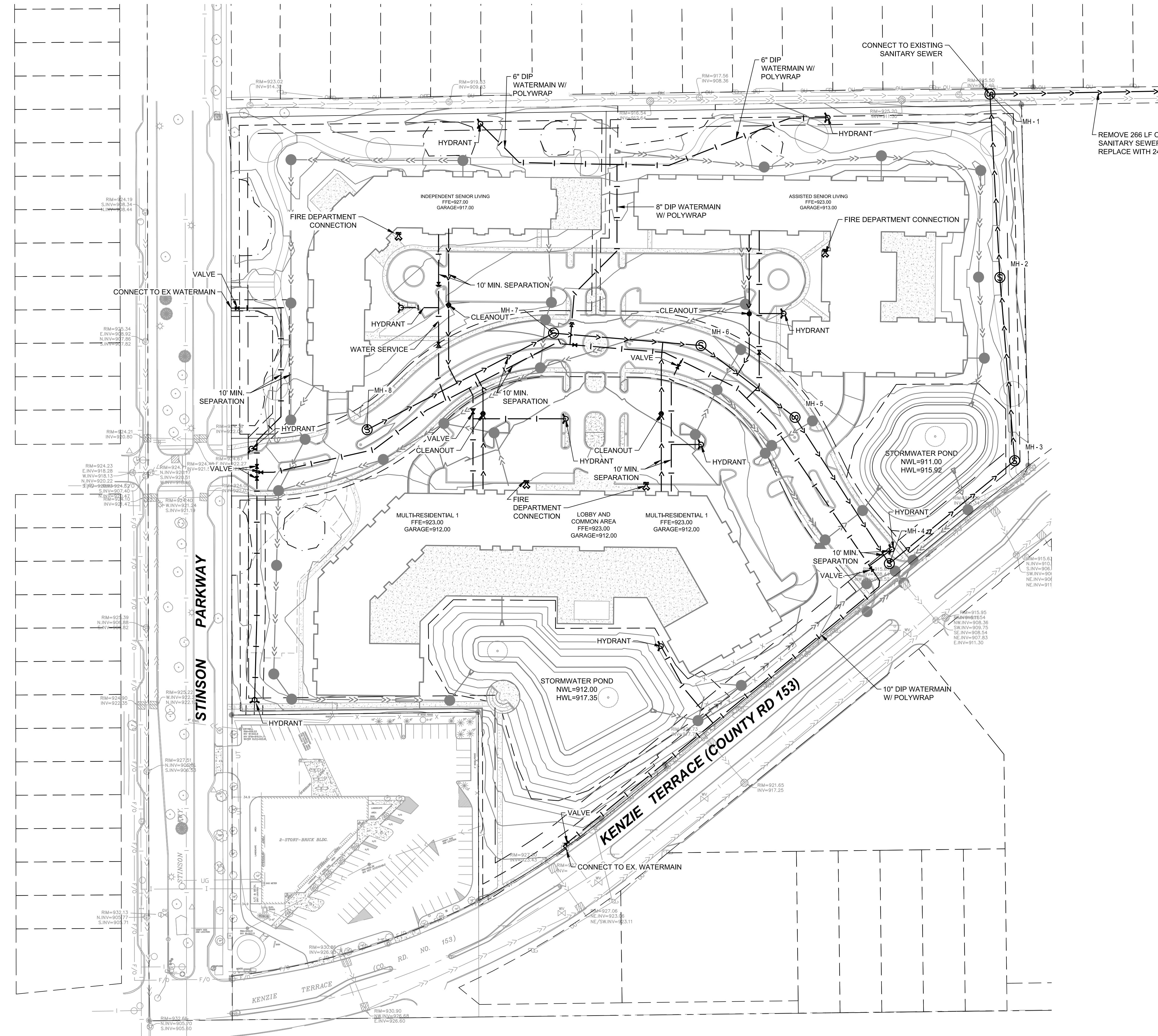
Re Date: 12/06/2017

ee #: 1

et #:

C-303

et Title:


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CALL BEFORE YOU DIG

GOPHER STATE ONE CALL

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166

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PREPARED BY ME OR UNDER
MY SUPERVISION AND IS A
DULY REGISTERED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE
STATE OF MINNESOTA.

License #:
Date:

Project #:

Drawn By:

Issue Date:

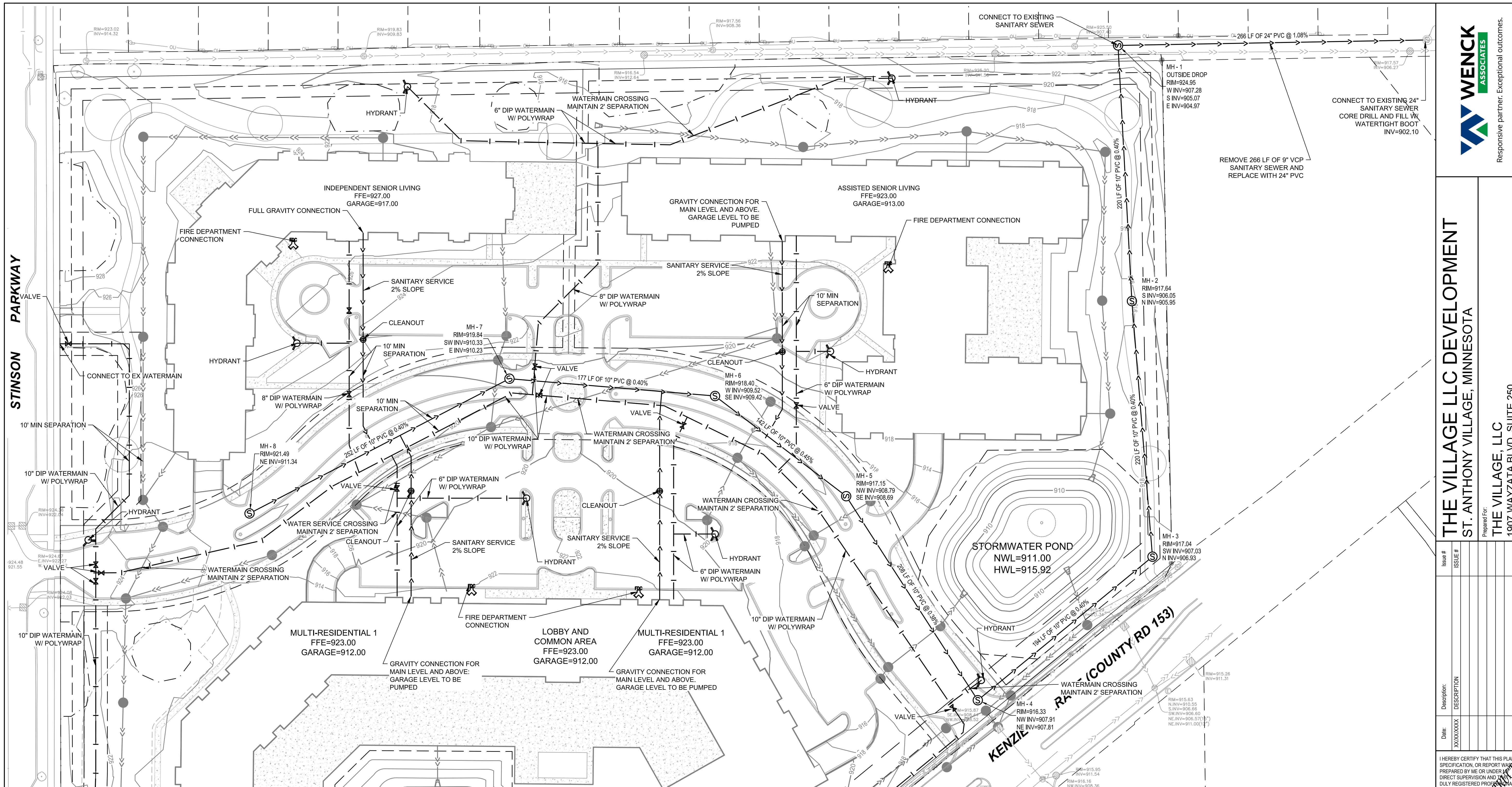
Issue #:

Sheet #:

C-401

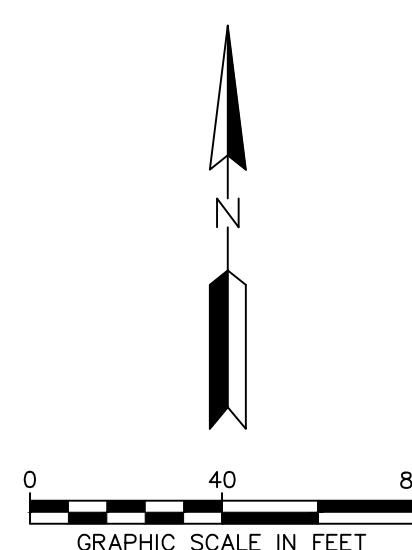
Sheet Title:

UTILITY PLAN



UTILITY LEGEND

	PROPERTY LINE
	SETBACK LINE
	EASEMENT LINE
	EXISTING SANITARY SEWER
	EXISTING / PROPOSED STORM SEWER
	EXISTING WATERMAIN
	EXISTING SANITARY STRUCTURE
	PROPOSED SANITARY STRUCTURE
	PROPOSED SANITARY SEWER
	PROPOSED WATERMAIN
	PROPOSED HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION



WARNING:

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CONSTRUCTION AT NO COST
CALL BEFORE YOU DIG

CALL BEFORE YOU DIG
GOPHER STATE ONE CALL
TWIN CITY AREA 251-454-2222

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166



WENCK
ASSOCIATES

responsive partner. Exceptional outcomes

THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

Prepared For:

ST. A
Prepared For:
THE
1907 W

I hereby certify that this plan, specification, or report was prepared by me or under my supervision and that I am a registered professional engineer under the laws of the state of Minnesota.

St. #: _____

116 **FOOT**

Ref. No. 50005 (1)

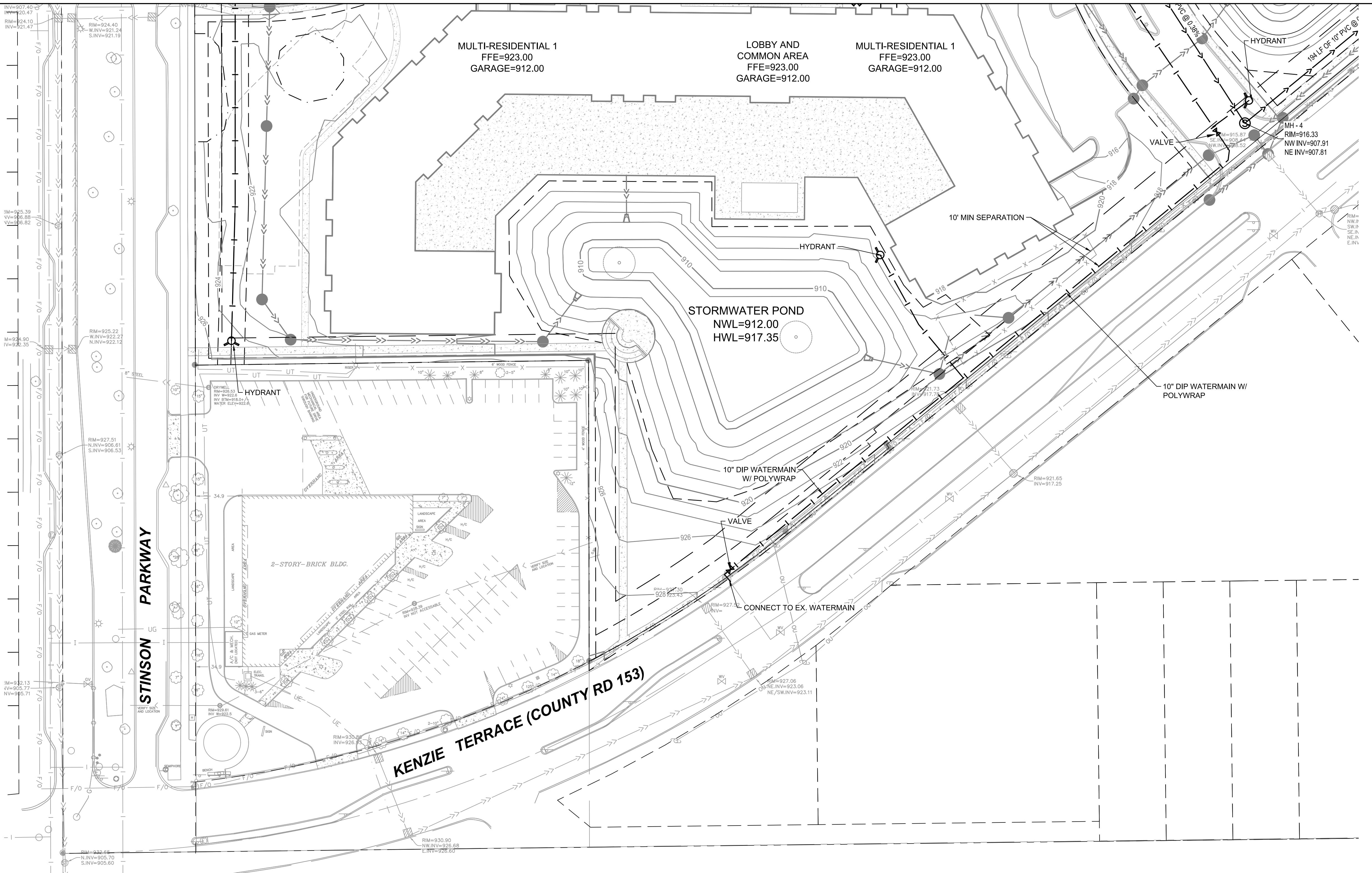
Date: 12/06/2012

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Unit #:

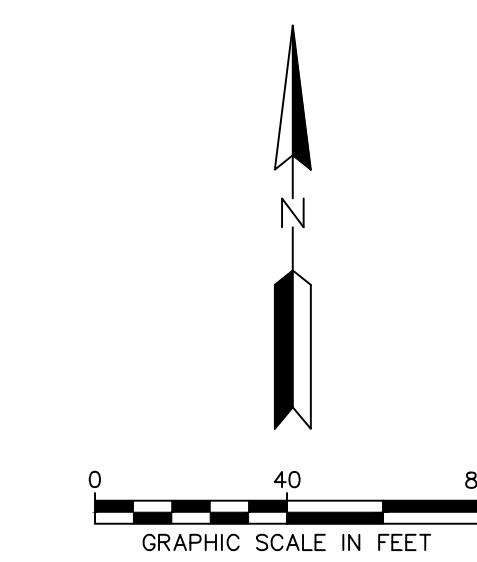
C-402

1 Title:



UTILITY LEGEND

—	PROPERTY LINE
— — —	SETBACK LINE
— — — — —	EASEMENT LINE
→ → →	EXISTING SANITARY SEWER
>>>	EXISTING / PROPOSED STORM SEWER
	EXISTING WATERMAIN
(S)	EXISTING SANITARY STRUCTURE
(S)	PROPOSED SANITARY STRUCTURE
→ → →	PROPOSED SANITARY SEWER
	PROPOSED WATERMAIN
(H)	PROPOSED HYDRANT
FDC	PROPOSED FIRE DEPARTMENT CONNECTION



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CONSTRUCTION AT NO COST
CALL BEFORE YOU DIG

CALL BEFORE YOU DIG
GOPHER STATE ONE CALL
TWIN CITY AREA: 651.454.0002

TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166

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NSA #:

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own By: **RAG**

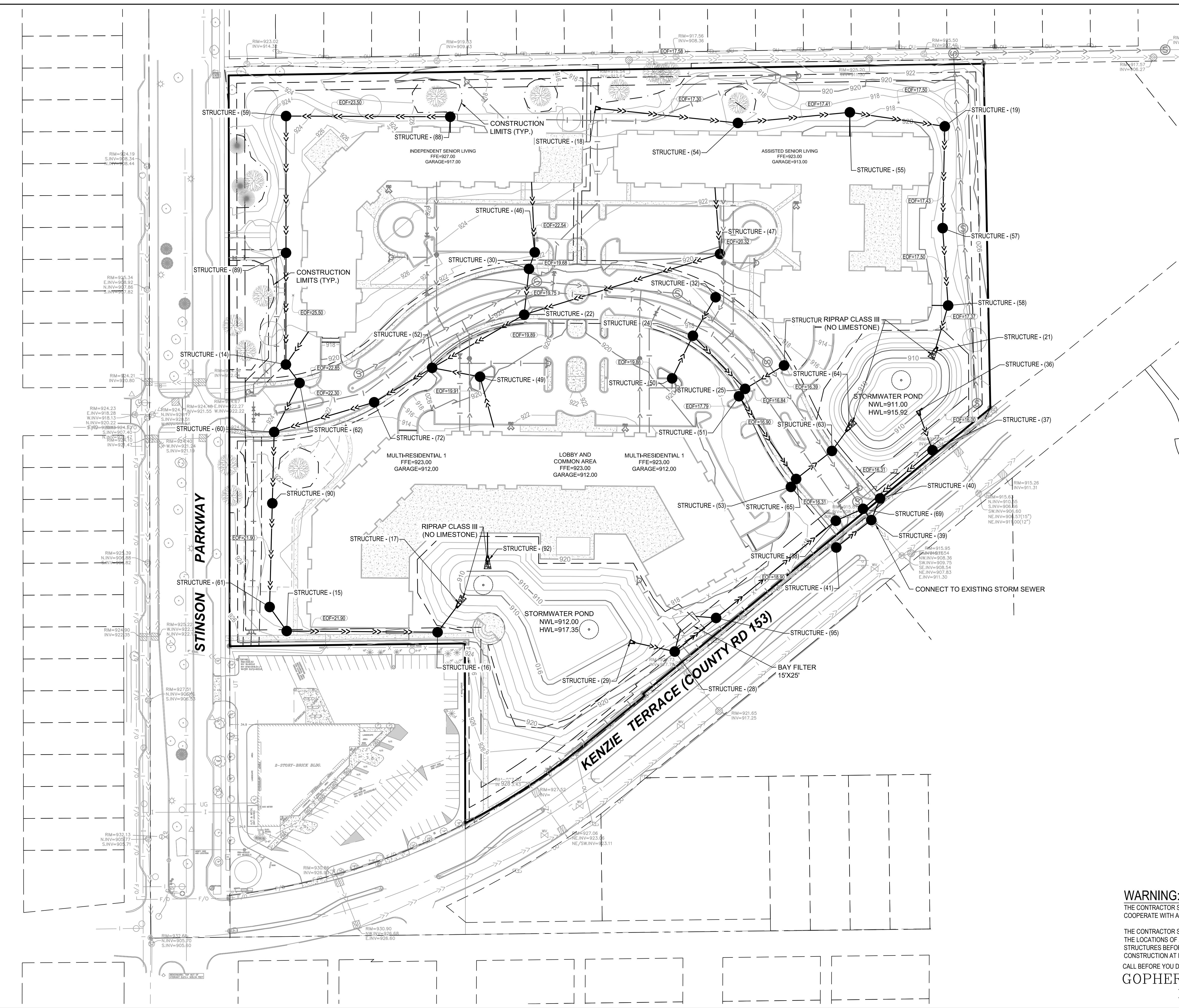
Re Date: 12/06/2017

e #: 1

et #:

C-403

et Title:



THE VILLAGE LLC DEVELOPMENT

ST. ANTHONY VILLAGE, MINNESOTA

Prepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391



Responsive partner. Exceptional outcomes.

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License #:
Date:

Project #:
5005-0003

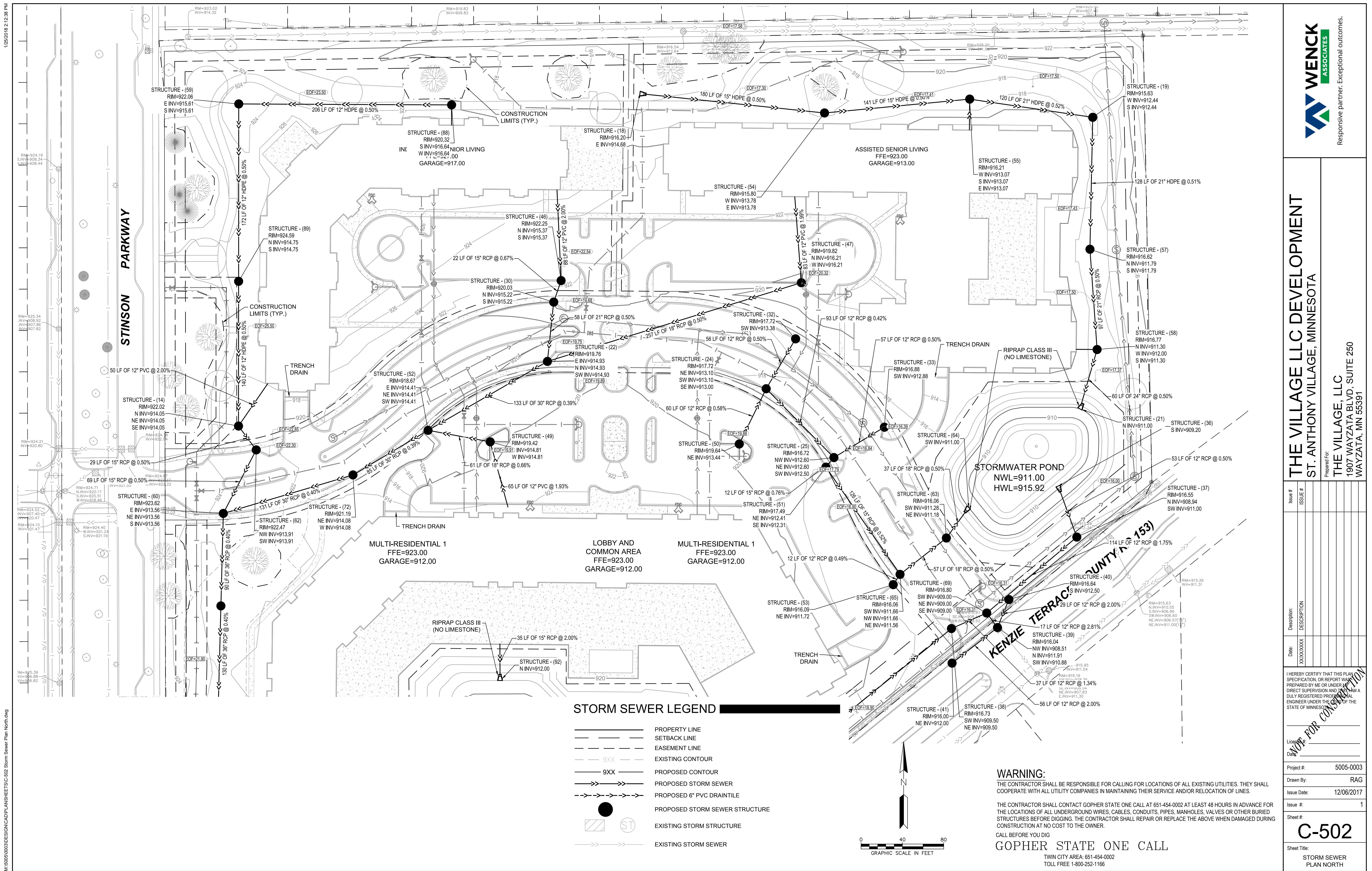
Drawn By:
RAG

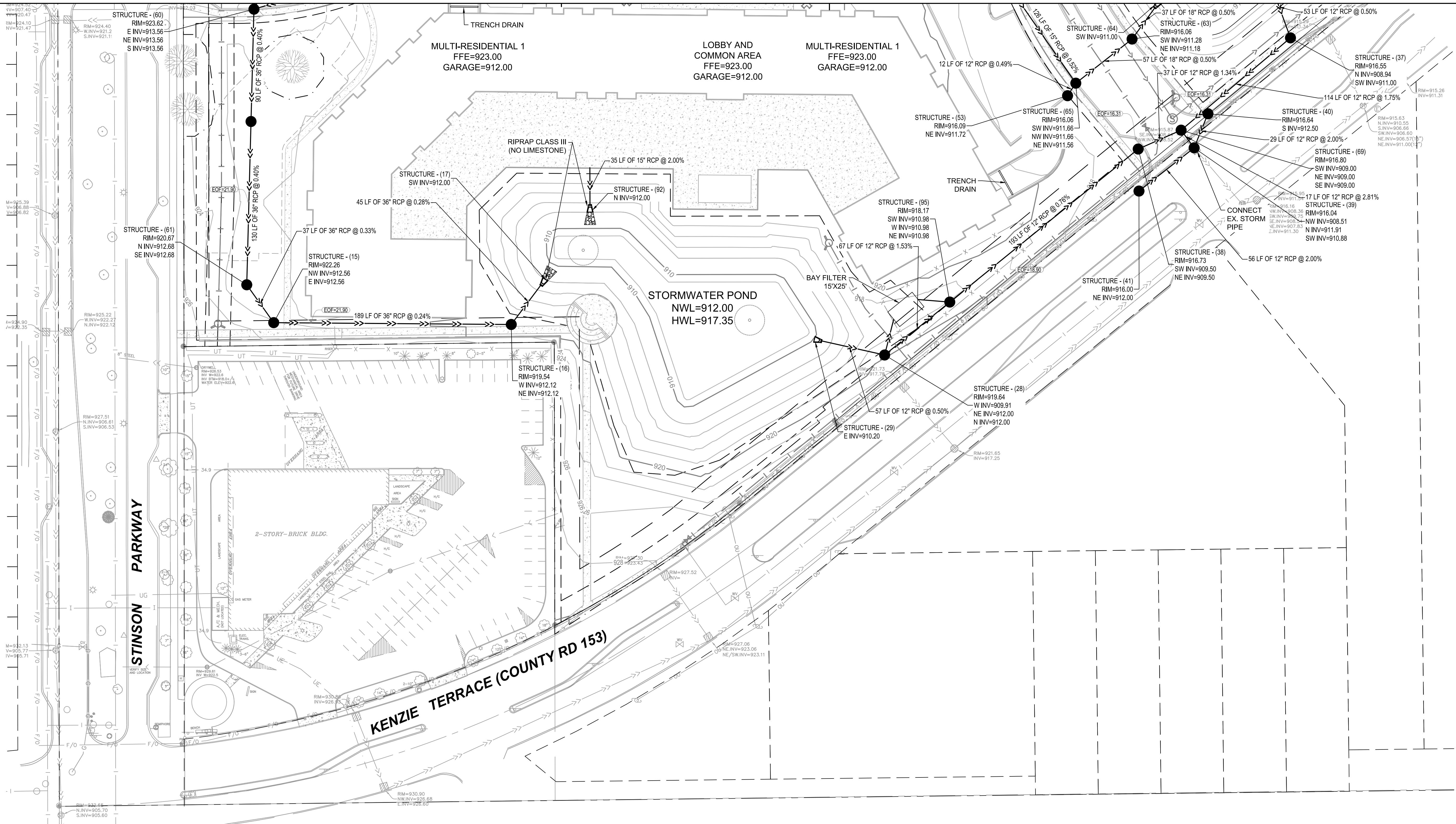
Issue Date:
12/06/2017

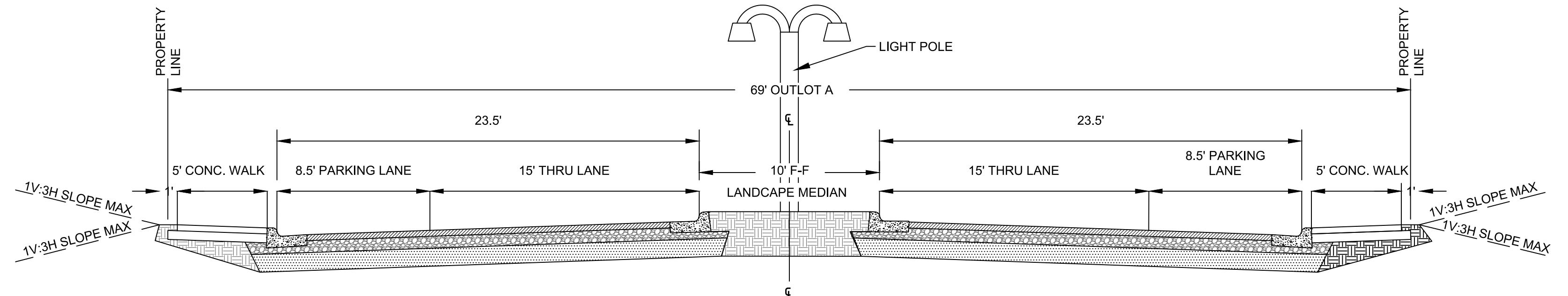
Issue #:
1

Sheet #:
C-501

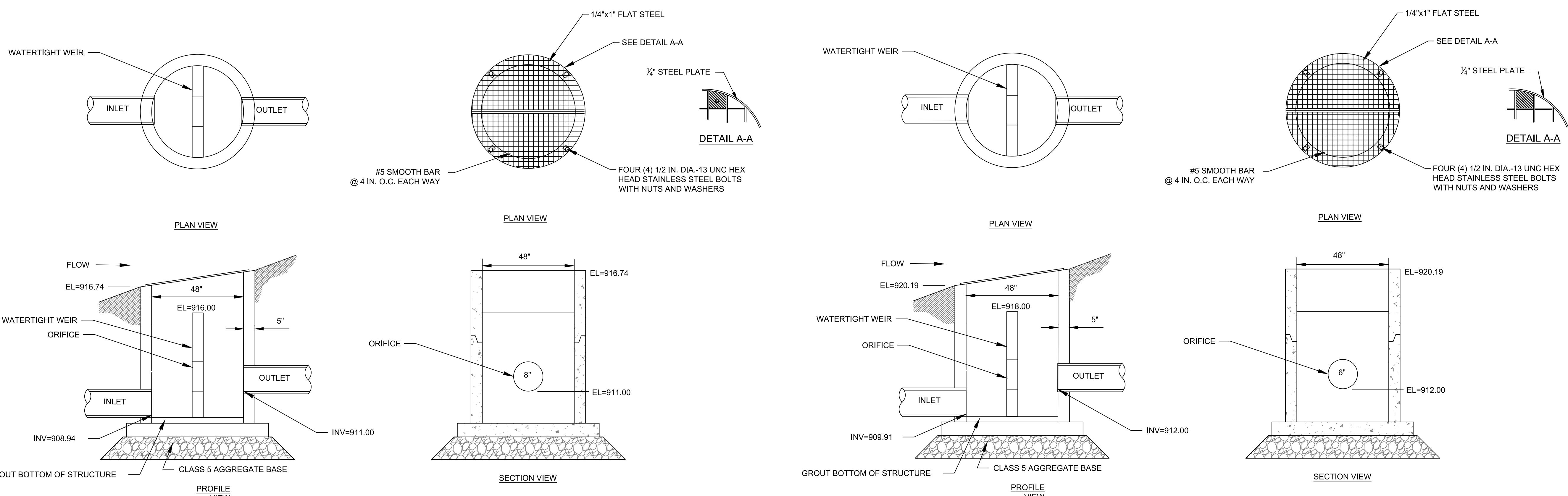
Sheet Title:
STORM
SEWER PLAN





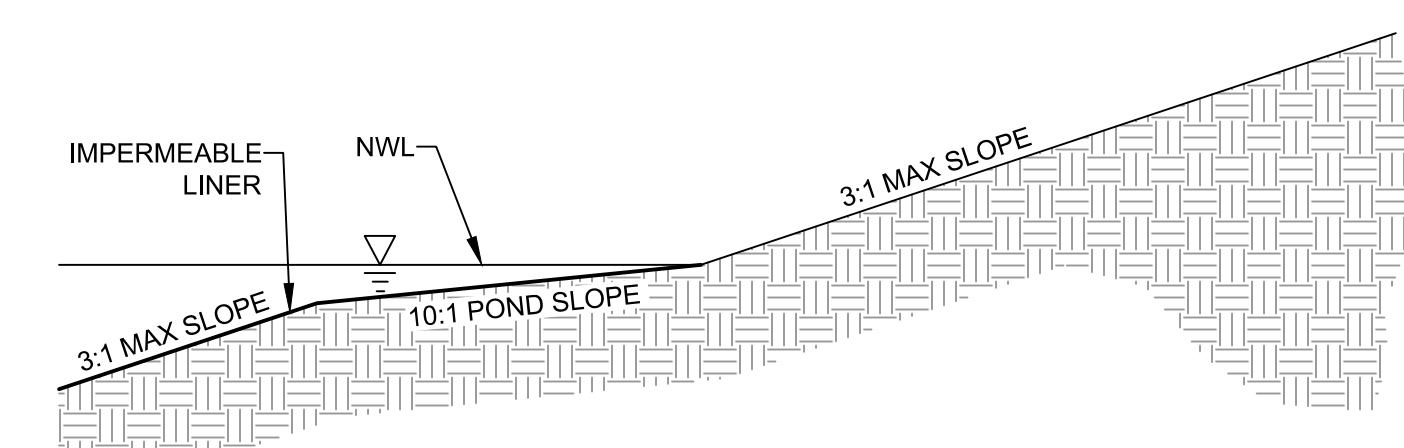


1 TYPICAL PARKWAY SECTION
C-801 NOT TO SCALE

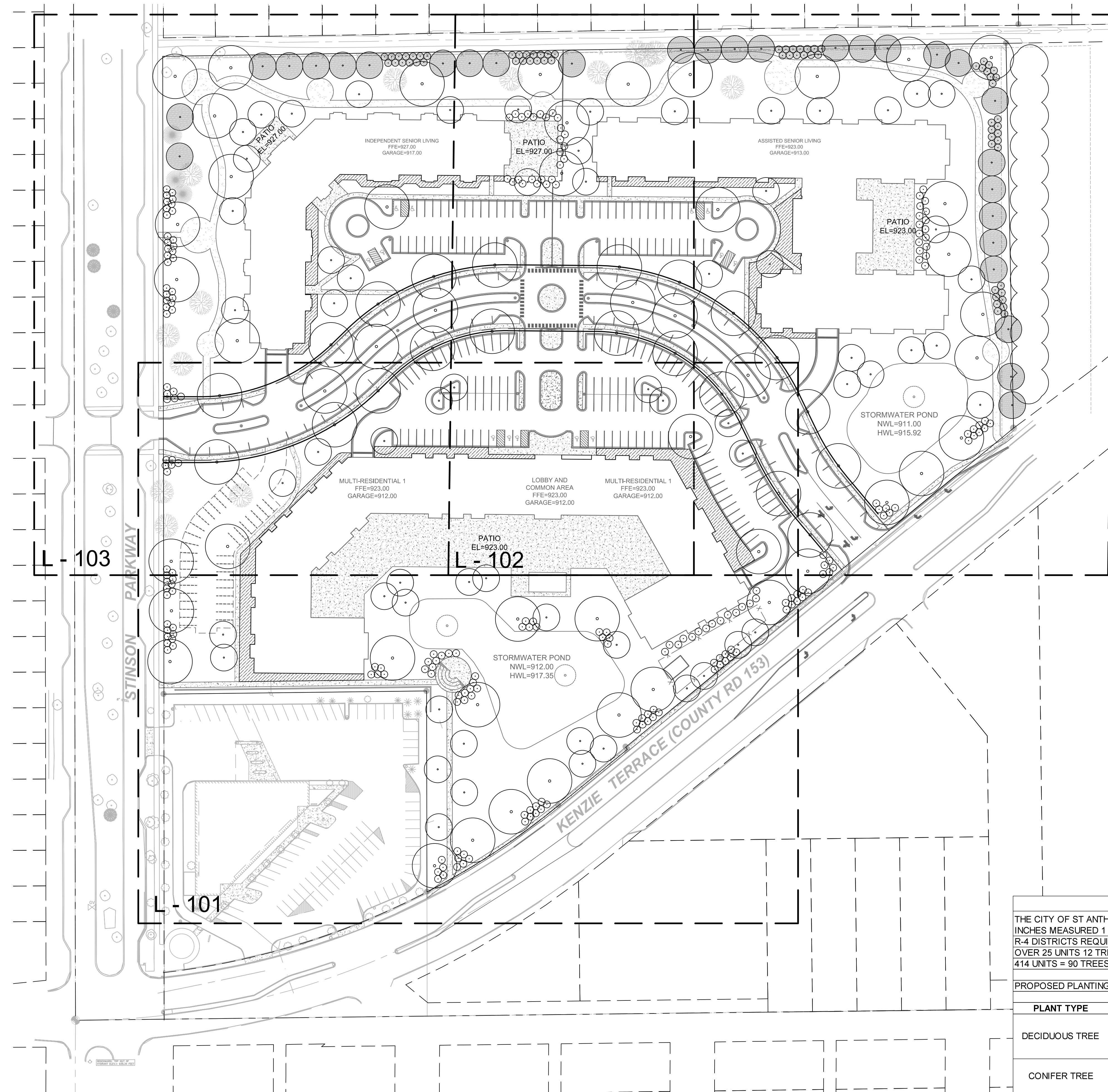


2 OUTLET CONTROL STRUCTURE (OCS) NORTH POND (STRUCTURE 37)
C-801 NOT TO SCALE

3 OUTLET CONTROL STRUCTURE (OCS) SOUTH POND (STRUCTURE 28)
C-801 NOT TO SCALE



4 TYPICAL POND SECTION
C-801 NOT TO SCALE



PLANTING SCHEDULE

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PROPOSED PLANTINGS:

PLANT TYPE	SYMBOL	QTY	COMMON NAME	SCIENTIFIC NAME	HEIGHT	WIDTH	SPACING	SIZE
DECIDUOUS TREE	Af	27	AUTUMN BLAZE MAPLE	<i>Acer x freemanii 'Jeffersred'</i>	50'	40'	PER PLAN	2" CAL.
	Co	18	HACKBERRY	<i>Celtis occidentalis</i>	50'	50'	PER PLAN	2" CAL.
	Qb	22	SWAMP WHITE OAK	<i>Quercus bicolor</i>	50'	50'	PER PLAN	2" CAL.
CONIFER TREE	Ac	13	CONCOLOR FIR	<i>Abies concolor</i>	30'	30'	PER PLAN	6'
	Pg	14	BLACK HILLS SPRUCE	<i>Picea glauca densata</i>	40'	30'	PER PLAN	6'
ORNAMENTAL TREE	N/A	69	CRABAPPLE/LILAC		20'	20'	PER PLAN	2" CAL.
		163						

THE VILLAGE LLC DEVELOPMENT
ST. ANTHONY VILLAGE, MINNESOTA

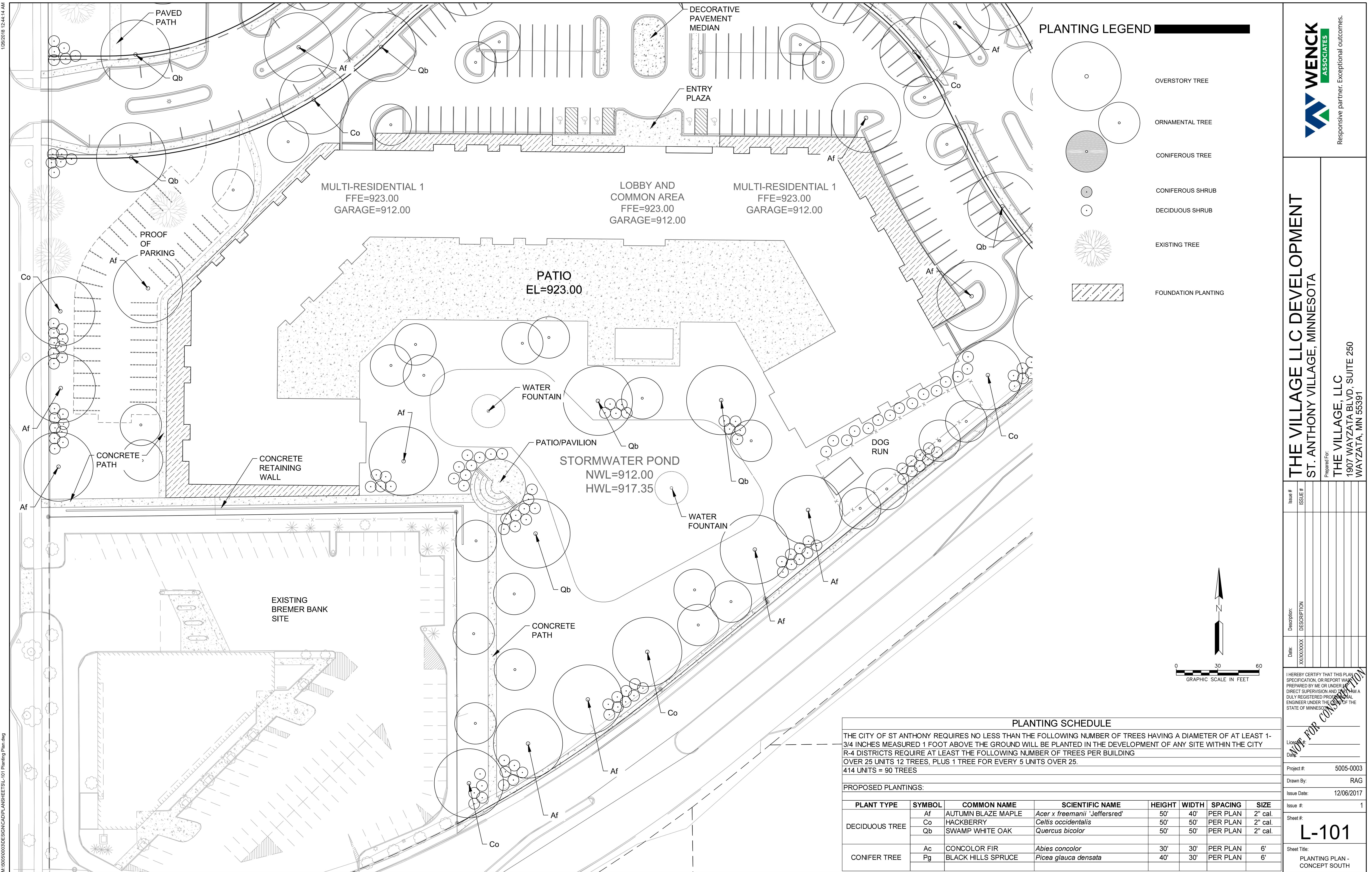
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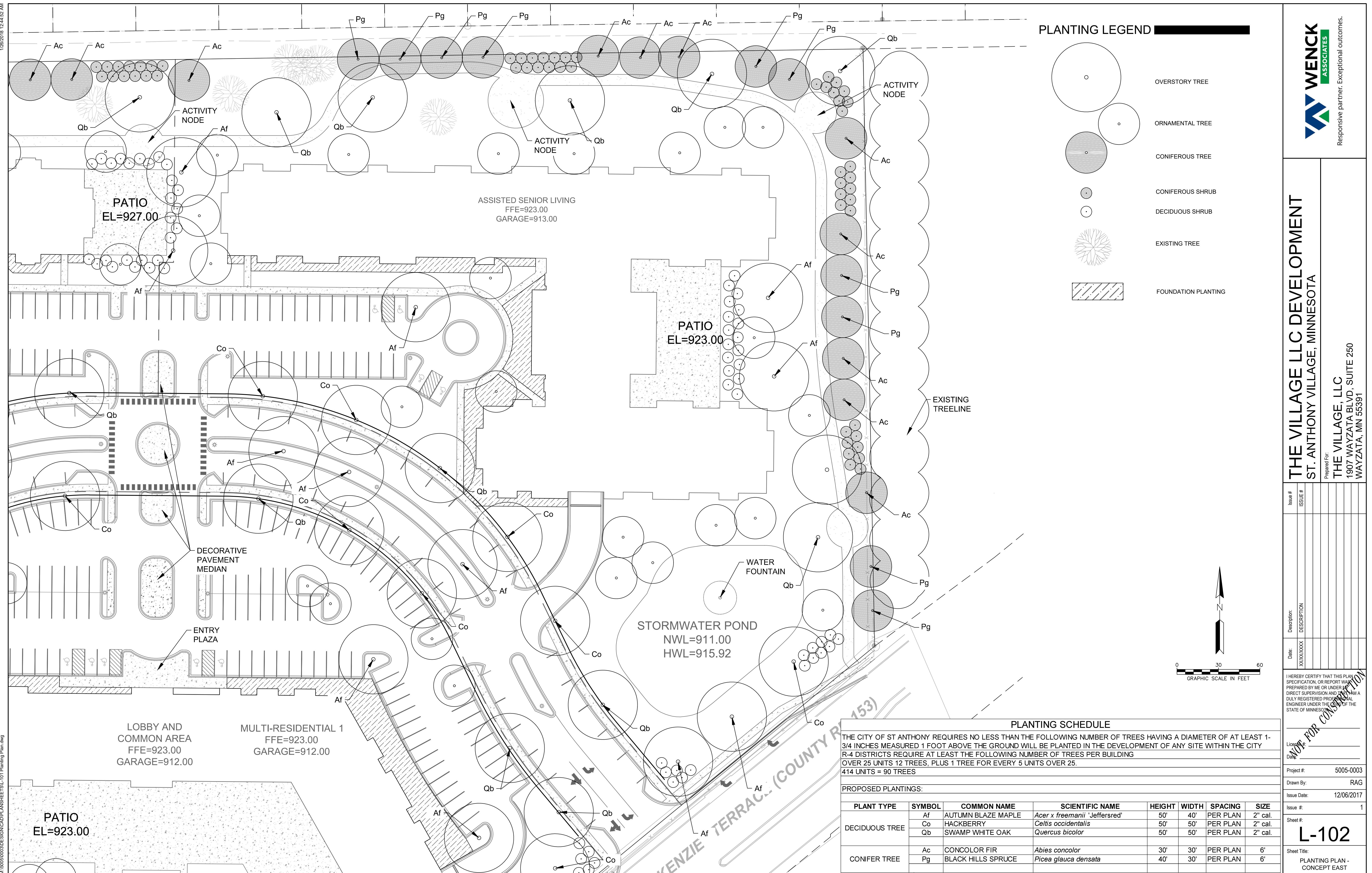
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

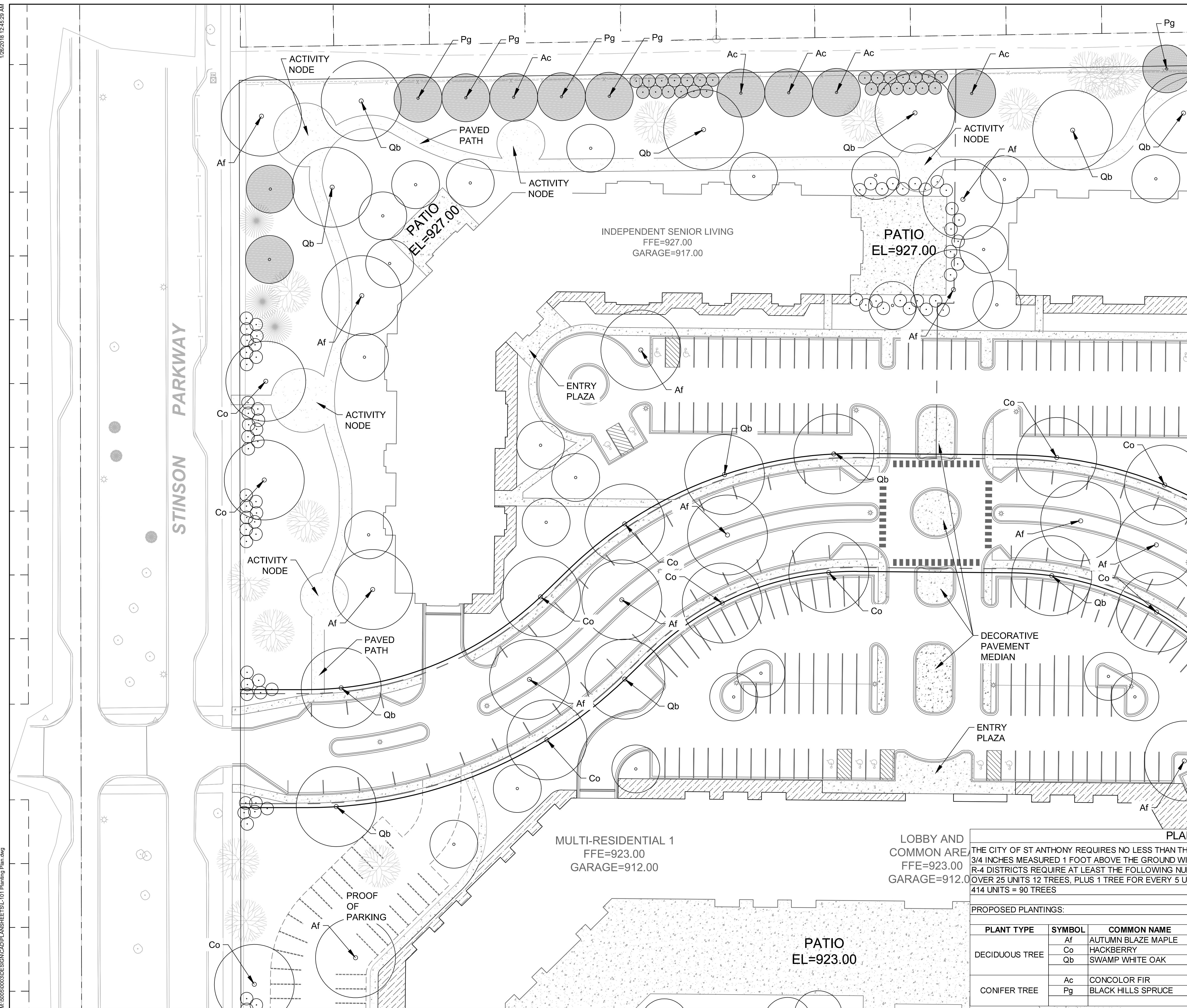
WENCK
ASSOCIATES

Responsive partner. Exceptional outcomes.

NOT FOR CONSTRUCTION
Sheet Title: PLANTING PLAN - OVERALL CONCEPT
Sheet #: L-100
Issue #: 1
Issue Date: 12/06/2017
Drawn By: RAG
Project #: 5005-0003
Lic.#: _____
Date: _____





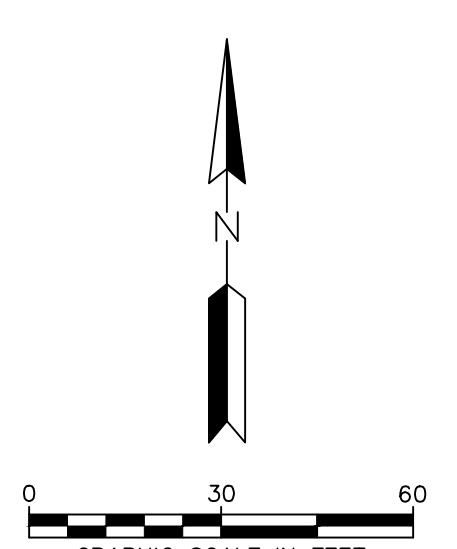


PLANTING LEGEND

THE VILLAGE LLC DEVELOPMENT
ST. ANTHONY VILLAGE, MINNESOTA

Prepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

Prepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391



I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND IS A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

License #:
Date:

Project #:
5005-0003

Drawn By:
RAG

Issue Date:
12/06/2017

Issue #:
1

Sheet #:
L-103

Sheet Title:
PLANTING PLAN - CONCEPT WEST

NON FOR CONSTRUCTION

**Revised Traffic Study
Southern Gateway
(The Village LLC)**

For:



**City of Saint Anthony Village
3301 Silver Lake Road
Saint Anthony, MN 55418**

February 19, 2018

Prepared By:

**WSB & Associates, Inc.
701 Xenia Ave. South, Suite 300
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(763) 541-4800
(763) 541-1700 (Fax)**

CERTIFICATION

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly registered professional engineer under the laws of the State of Minnesota.



Charles T. Rickart, P.E.

Date: February 19, 2018

Reg. No. 26082

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INTRODUCTION / BACKGROUND

A Traffic Impact Study, dated November 11, 2016, was completed for The Village LLC development as part of the Environmental Assessment Worksheet (EAW) process. The purpose of the study was to determine the potential transportation impacts for the redevelopment of the existing 15-acre, 200 unit manufactured home/RV park community, located in the northeast quadrant of the intersection of Kenzie Terrace (CR 153) and NE Stinson Parkway in the City of St Anthony. **Figure 1** and **Figure 2** show the project location and project area.

The development was originally proposed to include five, 3 to 5 story multi-unit buildings with a mix of apartments, senior living, and continuing care uses consisting of approximately 800 units and; 37, 2- to 3-story townhome units.

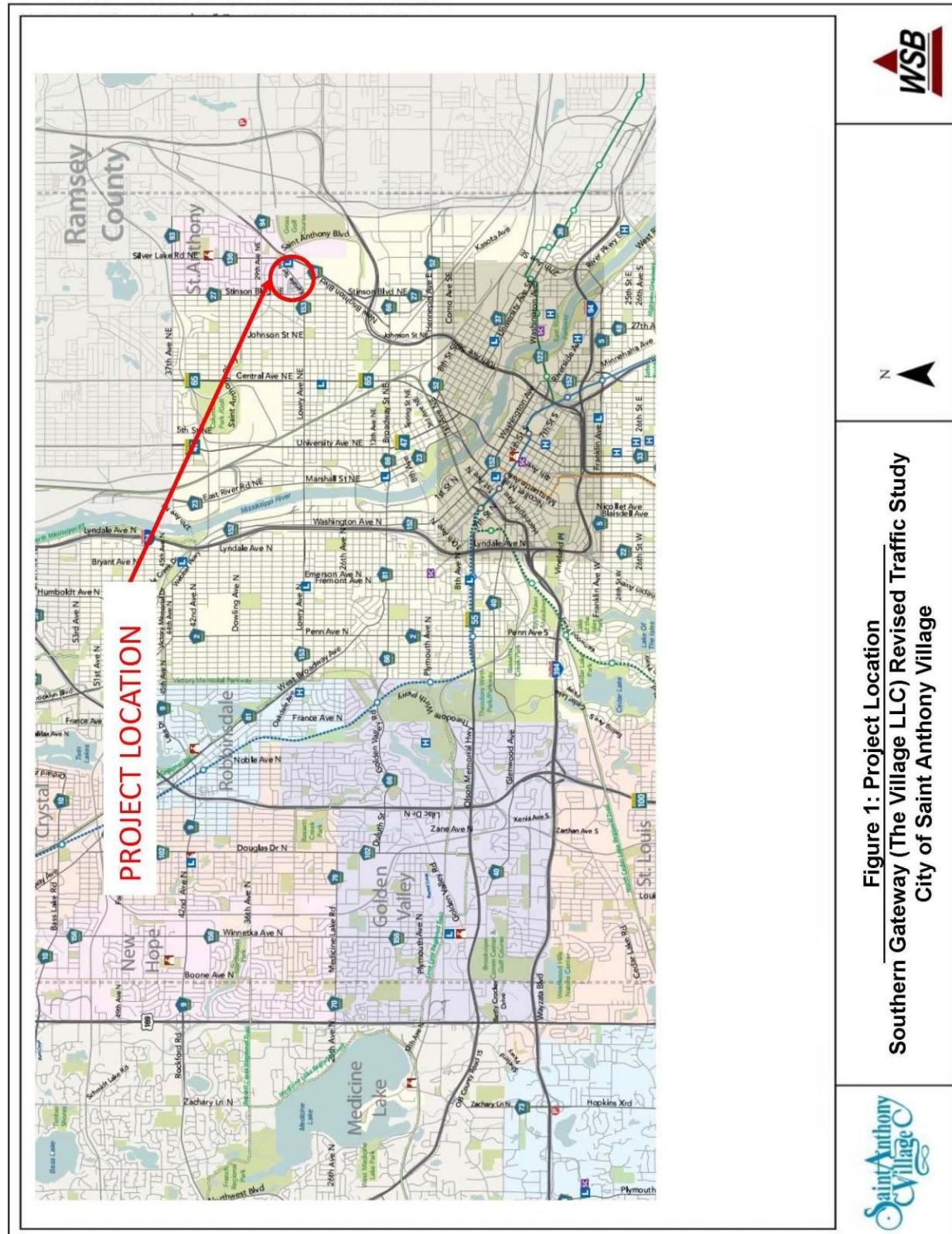
After input from the public and elected officials, the development team is now proposing a revised, less intense, development plan. The new plan now includes 225 market rate apartments; 61 senior living units; and, 128 assisted living units. The proposed site plan is shown on **Figure 3**.

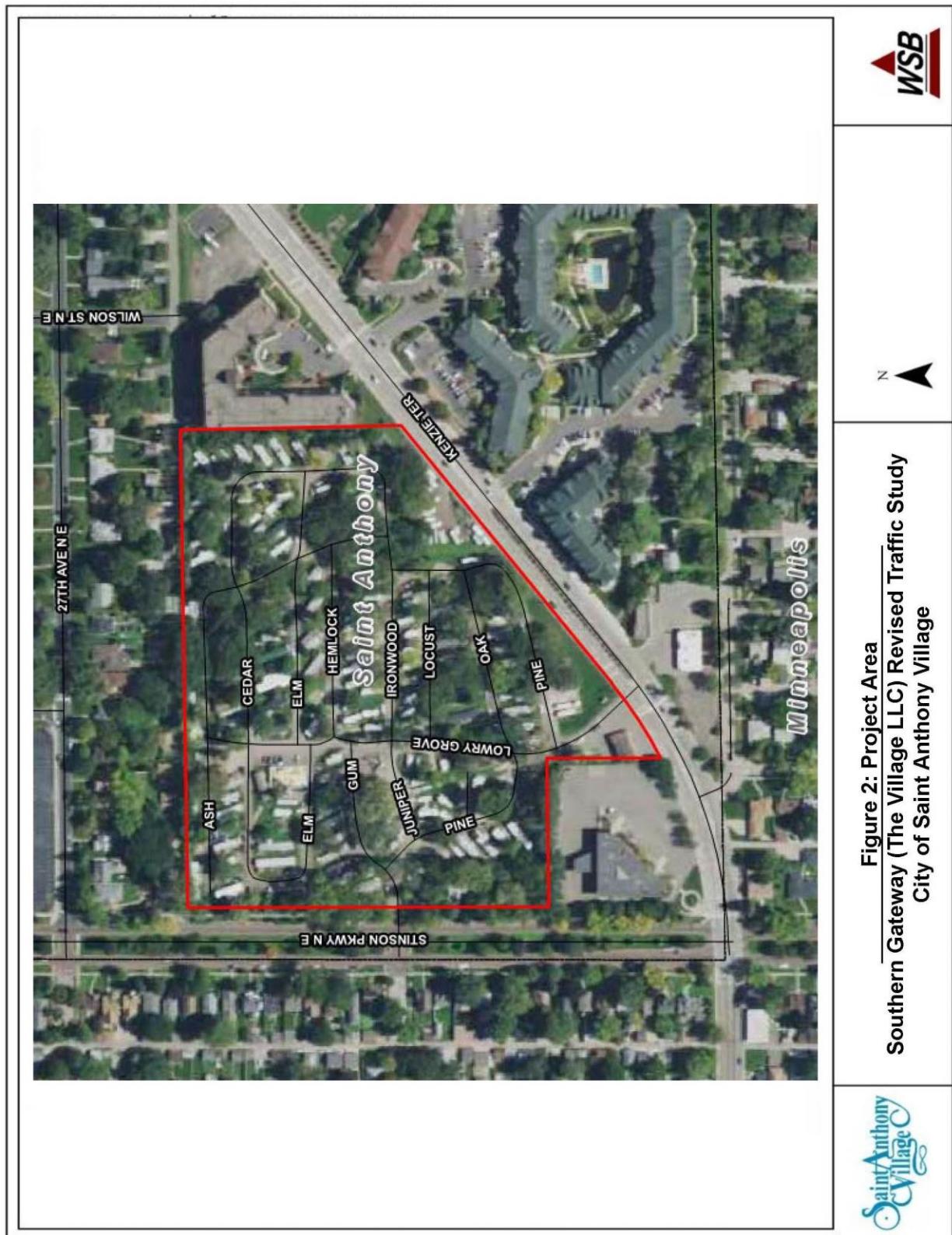
Based on the revised plan the transportation impacts of the existing and proposed traffic conditions were evaluated for the NE Stinson Parkway and Kenzie Terrace (CR 153)/Lowry Avenue corridors including the following intersections:

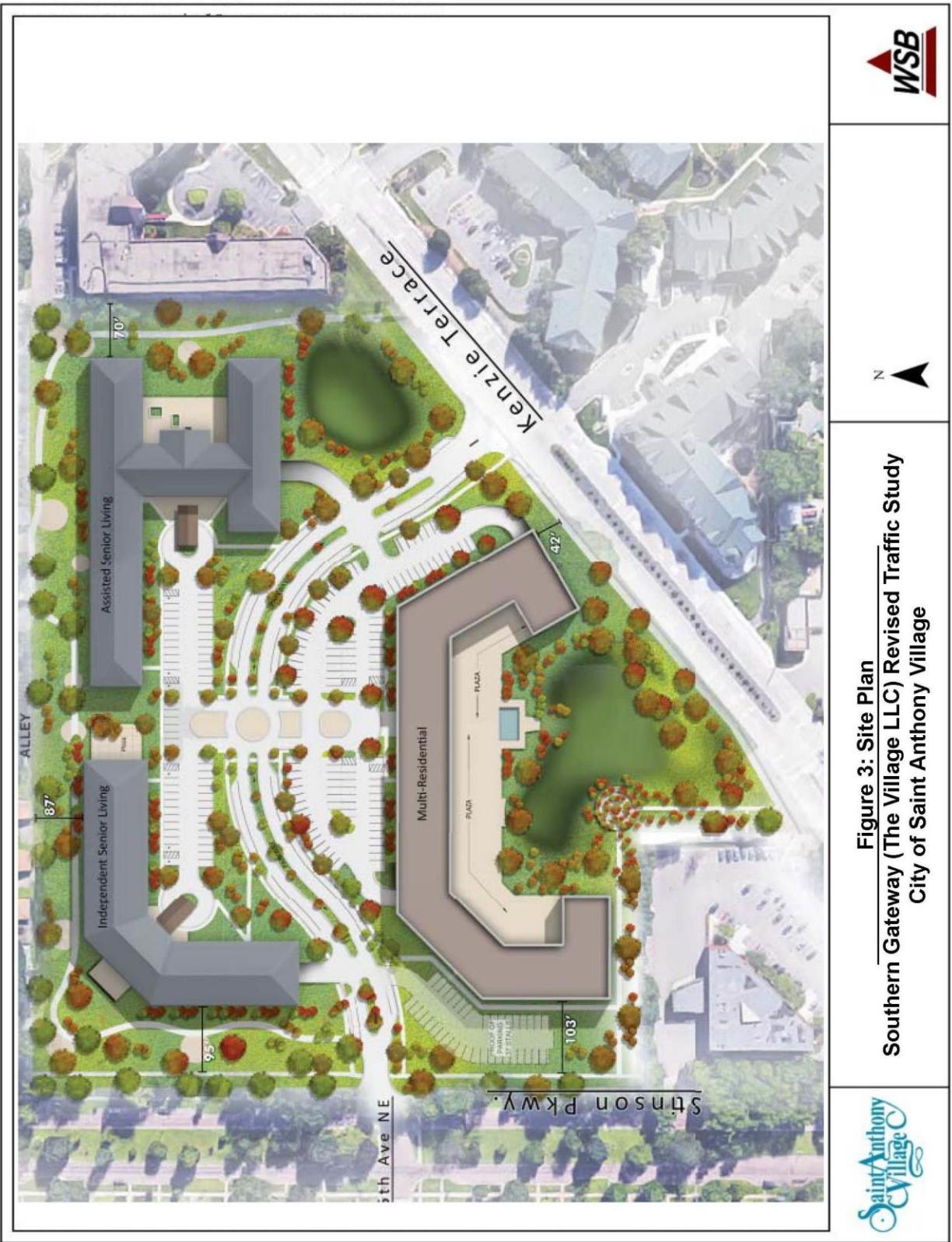
- St Anthony Blvd at NE Stinson Pkwy
- St Anthony Blvd at Silver Lake Blvd (CR 136) / Kenzie Terrace (CR 153)
- St Anthony Blvd at New Brighton Blvd (CR 88)
- NE Stinson Pkwy at 27th Avenue
- NE Stinson Pkwy at 26th Avenue
- NE Stinson Pkwy at Lowry Ave / Kenzie Terrace (CR 153)
- Kenzie Terrace (CR 153) at Lowry Grove Entrance
- Kenzie Terrace (CR 153) at Wilson Street
- Kenzie Terrace (CR 153) at Autumn Woods/Kenzington Apartments

The Traffic Engineering Review and Analysis in this Study was conducted to determine the impacts the site would have on the area traffic operations, lane geometry/traffic control and access. The following sections of this report outline the findings of this study including:

- Documentation of existing traffic conditions in the area, and;
- Projecting future traffic for the proposed development, and;
- Determining lane configuration needs at the primary driveways and intersections in the area, and;
- Developing conclusions and recommendations with respect to the existing and future roadway needs and configurations at area intersections and/or roadways.







EXISTING CONDITIONS

A. Land Use

The existing land use in the study area consists primarily of single family and multi-family residential, office commercial and retail uses.

B. Roadway Characteristics

Stinson Parkway is a north/south “A” Minor Arterial extending from 40th Avenue on the north, in Columbia Heights to E Hennepin Avenue in the City of Minneapolis. Stinson Parkway south of St Anthony Blvd, adjacent to the proposed site is a Minneapolis Park Board street and has a typical 95 foot urban cross section with one lane in each direction, a parking bay and a raised grass median. Traffic signal control is provided at the Lowry Ave/Kenzie Terrace (CR 153) intersection. All other intersections are Stop Sign controlled. A 25mph speed limit is posted on Stinson Parkway south of St Anthony Blvd.

Kenzie Terrace (CR 153) is a east/west “B” Minor Arterial extending from where it changes to Lowry Avenue at Stinson Parkway to where it changes to Silver Lake Road at St Anthony Parkway. Kenzie Terrace (CR 153) has a primary 64 foot two-lane urban cross section with a center left turn lane and a raised concrete median. It provides access to the multi-family apartments and businesses adjacent to the roadway with median breaks at several driveways. Traffic signal control is provided at the Stinson Parkway, Autumn Woods/Kenzington Apartments and St Anthony Blvd intersections. All other intersections are Stop Sign controlled. Signed and marked pedestrian crossings are provided at Stenson Parkway, at the Lowry Grove entrance, at Autumn Woods/Kenzington Apartments entrance, at 27th Avenue and at St Anthony Blvd. A 30mph speed limit is posted on Kenzie Terrace (CR 153).

The lane configurations at each of the study area intersections are as follows:

St Anthony Blvd at NE Stinson Pkwy – All Way Stop Control

NB Stinson Pkwy – one left/through/right, one parking lane

SB Stinson Pkwy – one left/through/right, one parking lane

WB St Anthony Blvd – one left/through/right

EB St Anthony Blvd – one left/through/right

St Anthony Blvd at Silver Lake Rd (CR 136) / Kenzie Terrace (CR 153) – Traffic Signal Control:

NW St Anthony Blvd – one left, one through, one free right turn lane

SE St Anthony Blvd – one left, one through/free right lane

SW Silver Lake Rd – one left, one through, one through/free right turn lane

NE Kenzie Terrace – one left, two through, one right

St Anthony Blvd at New Brighton Blvd (CR 88) – Traffic Signal Control:

NW St Anthony Blvd – one left, one through, one right
SE St Anthony Blvd – one left, one through, one free right
SW New Brighton Blvd – one left, two through, one right
NE New Brighton Blvd – one left, two through, one free right

NE Stinson Pkwy at 27th Avenue – All Way Stop Control:

NB Stinson Pkwy – one left/through/right
SB Stinson Pkwy – one left/through/right
WB 27th Avenue – one left/through/right
EB 27th Avenue – one left/through/right

NE Stinson Pkwy at 26th Avenue – Side Street Stop Control:

NB Stinson Pkwy – one left/through/right
SB Stinson Pkwy – one left/through/right
WB 26th Avenue – one left/through/right
EB 26th Avenue – one left/through/right

NE Stinson Pkwy at Lowry Ave / Kenzie Terrace (CR 153) – Traffic Signal Control:

NB Stinson Pkwy – one left, one through/right
SB Stinson Pkwy – one left, one through/right
WB Kenzie Terrace – one left, one through, one right
EB Lowry Ave – one left/through, one right

Kenzie Terrace (CR 153) at Lowry Grove Entrance – Side Street Stop Control:

SB Lowry Grove Entrance – one left/through/right
WB Kenzie Terrace – one left/through, one through/right
EB Kenzie Terrace – one left, one through/right

Kenzie Terrace (CR 153) at Wilson Street – Side Street Stop Control:

NB Wilson Street – one left/through/right
WB Kenzie Terrace – one left, one through, one through/right
EB Kenzie Terrace – one left/through, one through/right

Kenzie Terrace (CR 153) at Autumn Woods/Kenzington Apartments – Traffic Signal Control:

NB Autumn Woods – one left/through/right
SB Kenzington Apts – one left/through/right
WB Kenzie Terrace – one left, one through, one through/right
EB Kenzie Terrace – one left, one through, one through/right

The existing roadway geometrics including the intersection traffic control, is shown on **Figure 4** and **Figure 5**.

C. Traffic Volumes

AM and PM peak hour turning movements and daily counts were conducted during the week of October 10th, 2016. These counts were used as the existing baseline conditions for the area. **Figure 4** and **Figure 5** shows the existing intersections that were analyzed as part of this, with the existing 2016 AM and PM peak hour and traffic volumes.

D. Crash Data

The crash data included with this study was obtained using the Minnesota Crash Mapping Analysis Tool (MnCMAT) developed by MnDOT. The database includes crashes reported to MnDOT by local law enforcement agencies.

The crash data presented are for the years of 2013-2015. The MnCMAT database does not provide access to the original handwritten crash reports which contain some details that are not represented in the MnCMAT database. Crashes that resulted in damages under \$1000 may not be included in the database results as well.

The results indicate that the intersections of St Anthony Blvd at New Brighton Blvd, Stinson Blvd at 27th Street and Stinson Blvd at 26th Street have calculated accident and/or severity rates above the MnDOT Metro and Statewide averages.

A summary of the existing crash data is shown in **Table 1**.

Table 1 – Crash Summary

Intersection	2013		2014		2015		Total Crashes
	PD	PI	PD	PI	PD	PI	
St Anthony Blvd at Stinson Pkwy	1	0	0	0	0	0	1
St Anthony Blvd at Kenzie Terrace	2	0	2	0	2	0	6
St Anthony Blvd at New Brighton Blvd	4	2	4	1	3	2	16
Stinson Blvd at 27 th Street	1	0	0	0	1	1	3
Stinson Blvd at 26 th Street	0	1	0	0	0	0	1
Stinson Blvd at Lowry Ave/Kenzie Terrace	0	0	3	2	1	2	8
Kenzie Terrace at Lowry Grove Entrance	0	0	0	0	0	0	0
Kenzie Terrace at Wilson Street	0	0	0	0	0	0	0
Kenzie Terrace at Autumn Wood/Apartments	0	1	0	0	0	0	1

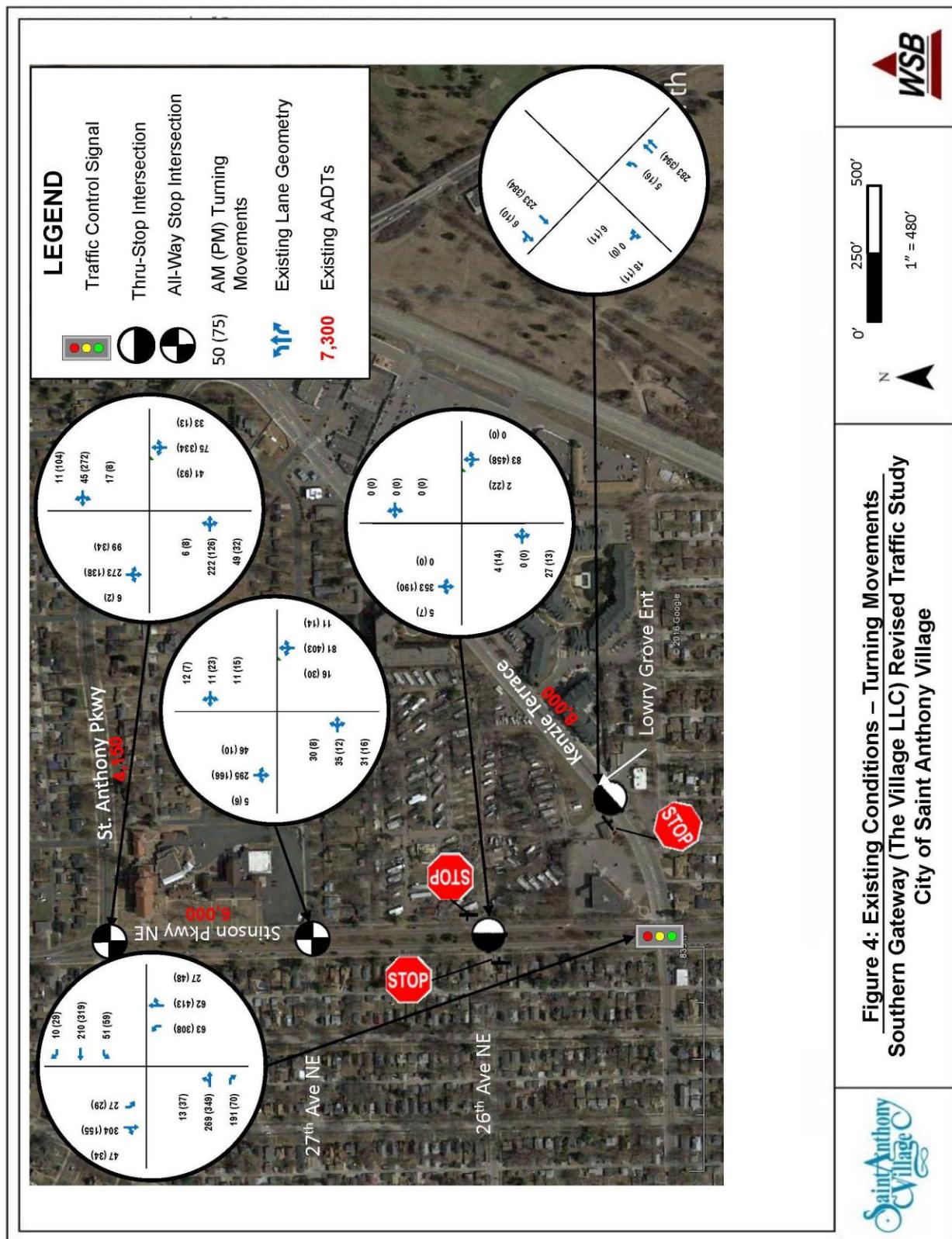
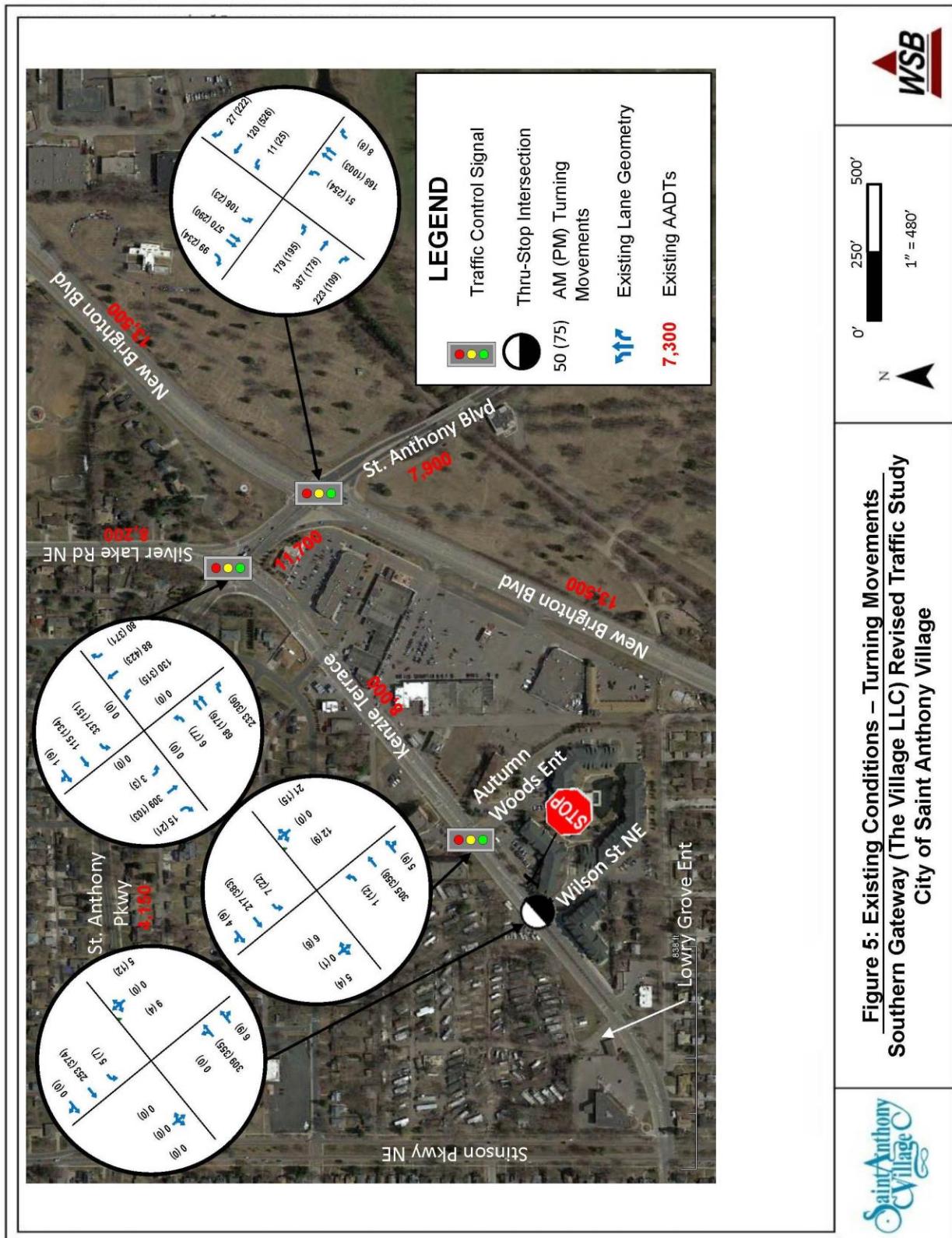


Figure 4: Existing Conditions – Turning Movements
Southern Gateway (The Village LLC) Revised Traffic Study
City of Saint Anthony Village



TRAFFIC PROJECTIONS

In order to analyze the lane configuration and traffic control needs, projected traffic volumes were determined for the area. Projected 2019 and 2030 traffic volumes were determined based on proposed anticipated future development land use in the area and the City's current Transportation Plan. The following sections outline the traffic generation from the study area, as well as the traffic distribution and projected traffic volumes.

A. *Traffic Generation*

Traffic growth in the vicinity of a proposed site will occur between existing conditions and any given future year due to other development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed somewhat constant or dropped in the past few years. However, in order to account for some background growth in traffic the Metropolitan Travel Demand model projection factor of 0.15% per year was used to project traffic from the 2016 counts to the 2019 and 2030 analysis years.

The estimated trip generation from the Southern Gateway (revised Village LLC) development project is shown in **Table 2**. The trip generation used to estimate the proposed site traffic is also based on rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual, 10th Edition*. The table shows the weekday AM and PM peak hour trip generation for the proposed development.

Table 2 - Estimated Southern Gateway Development Site Trip Generation

Planned Use	Dwelling Units	ADT			AM Peak			PM Peak		
		Total	In	Out	Total	In	Out	Total	In	Out
Apartments	225	1487	743	744	114	30	84	141	86	55
Senior Living	61	203	102	101	12	4	8	16	9	7
Townhomes	128	473	236	237	38	24	14	76	29	47
Total	414	2163	1081	1082	164	58	106	233	124	109

Source: Institute of Transportation Engineers Trip Generation Manual, 10th Edition

As discussed previously the proposed Southern Gateway Development is less intense than the original The Village LLC Development. **Table 3** below shows a comparison of the traffic generation that would be anticipated from the existing Mobile Home Park, assuming it fully developed; the original The Village LLC Development, and; the revised Southern Gateway Development.

Table 3 - Site Trip Generation Comparison

Option	Use	Size (Units)	ADT	AM Peak Hour	PM Peak Hour
Existing					
	Mobile Home Park	200	1298	82	118
The Village LLC Development					
	Apartments	500	3154	249	293
	Senior Living	100	319	20	26
	Continuing Care Retirement Community	200	640	47	85
	Townhomes	37	271	23	27
	Total	837	4384	339	431
Southern Gateway Development					
	Apartments	225	1487	114	141
	Senior Living	61	203	12	16
	Assisted Living	128	473	38	76
	Total	414	2163	164	233

B. Traffic Distribution

Area generated trips were distributed to the adjacent roadway system based on several factors including:

- Previous traffic and transportation studies in the area.
- Anticipated origins and destinations for specific land use (i.e. location of commercial uses in relationship to residential).
- Existing travel patterns and future roadway connections.

Based on these parameters the following general traffic distribution was used to distribute the projected traffic volumes in the study area:

- 5% to/from the north on Stinson Blvd (CR 27)
- 4% to/from north on Silver Lake Rd (CR 136)
- 5% to/from northeast on New Brighton Blvd (CR 88)
- 5% to/from southeast on St Anthony Blvd
- 20% to/from southwest on New Brighton Blvd (CR 88)
- 50% to/from south on Stinson Blvd
- 6% to/from west on Lowry Ave
- 1% to/from west on 26th Ave
- 1% to/from west on 27th Ave
- 3% to/from west on St Anthony Blvd

C. *Projected Traffic Volumes*

Traffic forecasts were prepared for the year 2019 which is the year after the initial phase the proposed development is anticipated to be completed and for the 2030 conditions the Comprehensive Plan year which represents the full development of the area.

The traffic forecasts were prepared by adding the projected annual background traffic growth and the projected non-development traffic growth to the existing 2016 traffic counts to determine the “No-Build” traffic conditions. It was determined that with the minimal increase in background traffic the 2019 no-build condition would be similar to the existing 2016 conditions, therefore it was not included in the analysis.

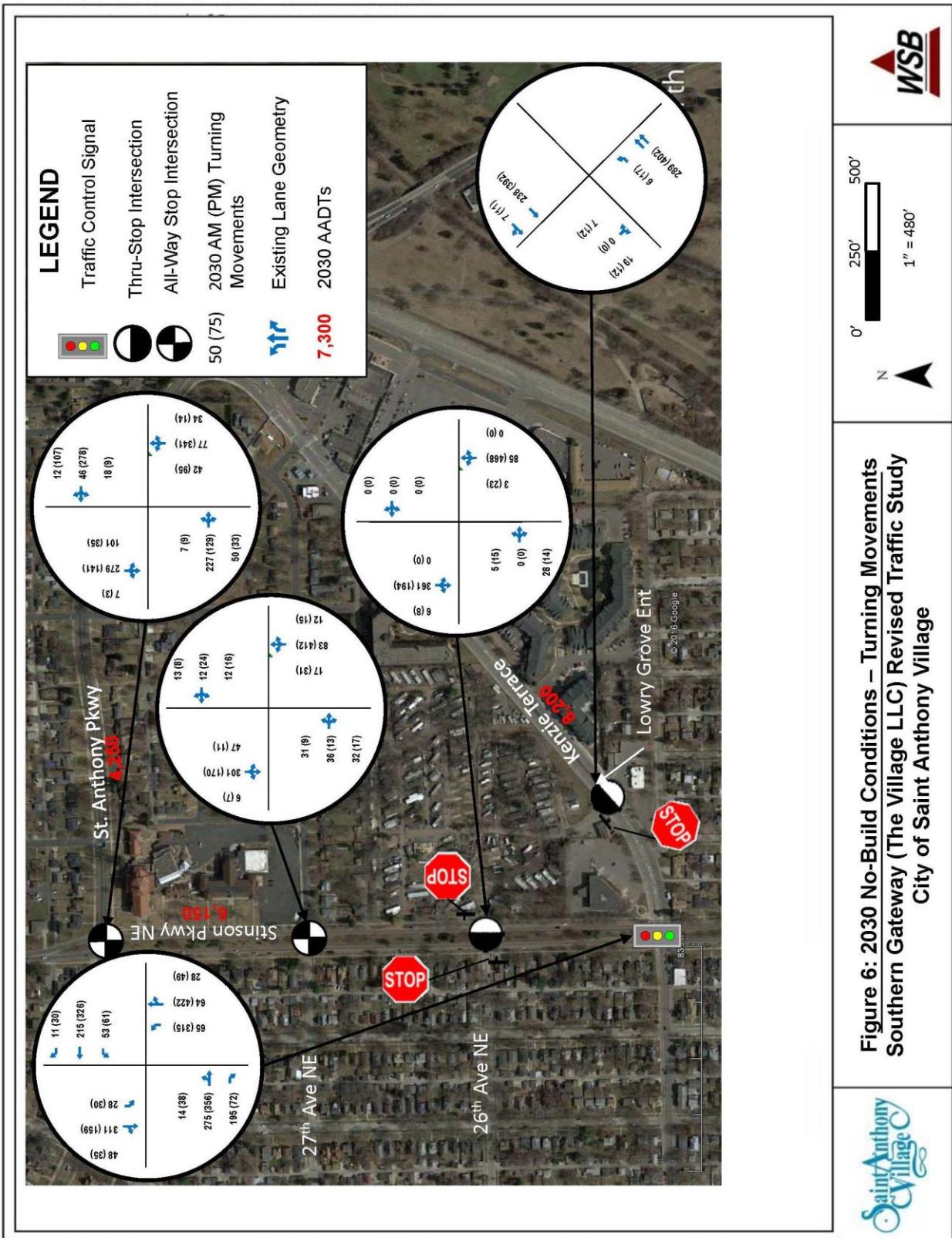
The proposed development traffic was then added to the no-build traffic conditions to determine the 2019 and 2030 Build traffic conditions. As a worst case condition it was assumed that the entire site would be developed for the 2019 build condition. **Figures 6 - 11** show the projected 2030 no-build, 2019 build and 2030 Build AM and PM peak hour traffic volumes.

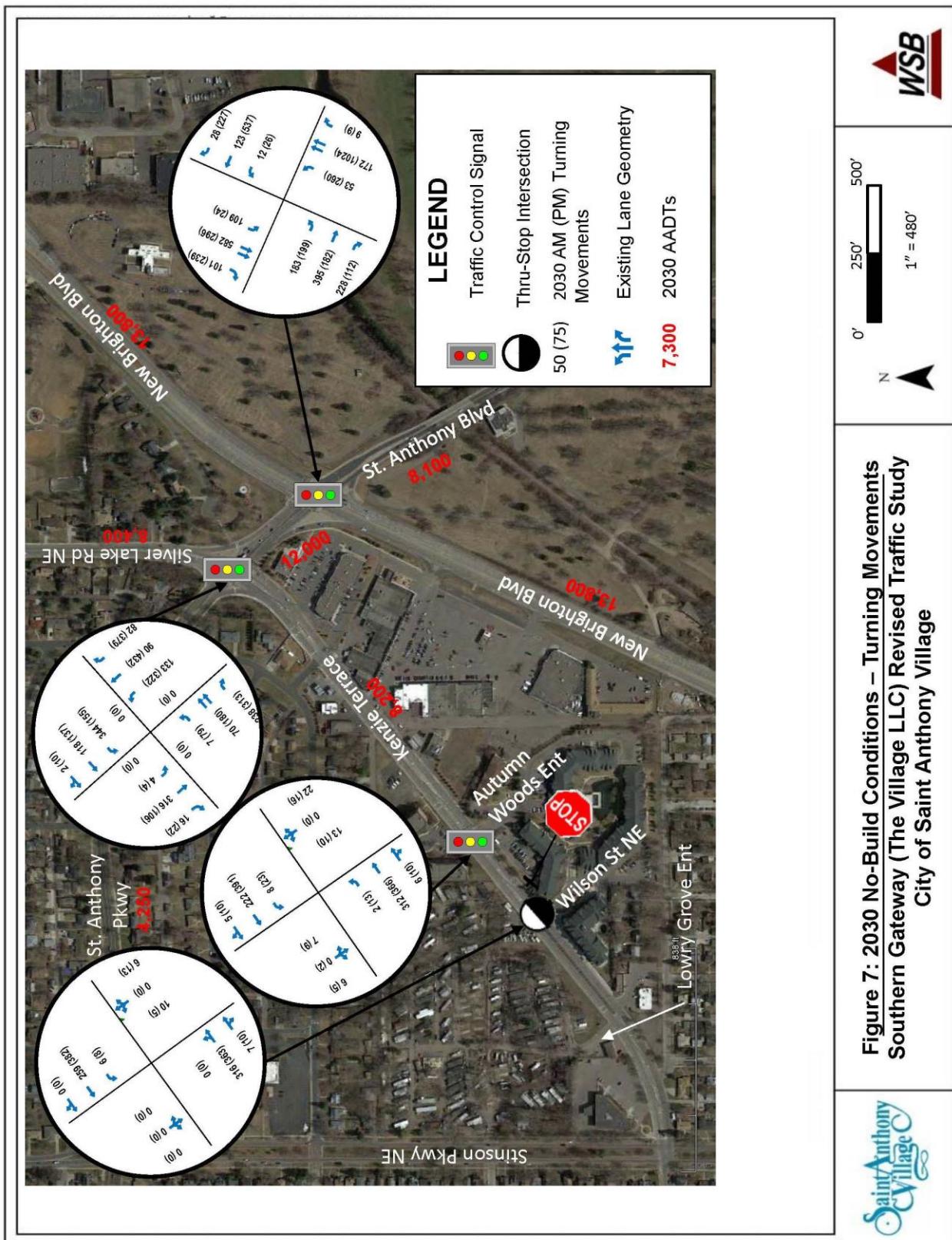
During the preparation of the previous Traffic Study concerns were raised with the amount of traffic on adjacent roadways, specifically; Stinson Parkway north and south of Kenzie Terrace (CR 153) and Lowry Avenue west of Stinson Parkway.

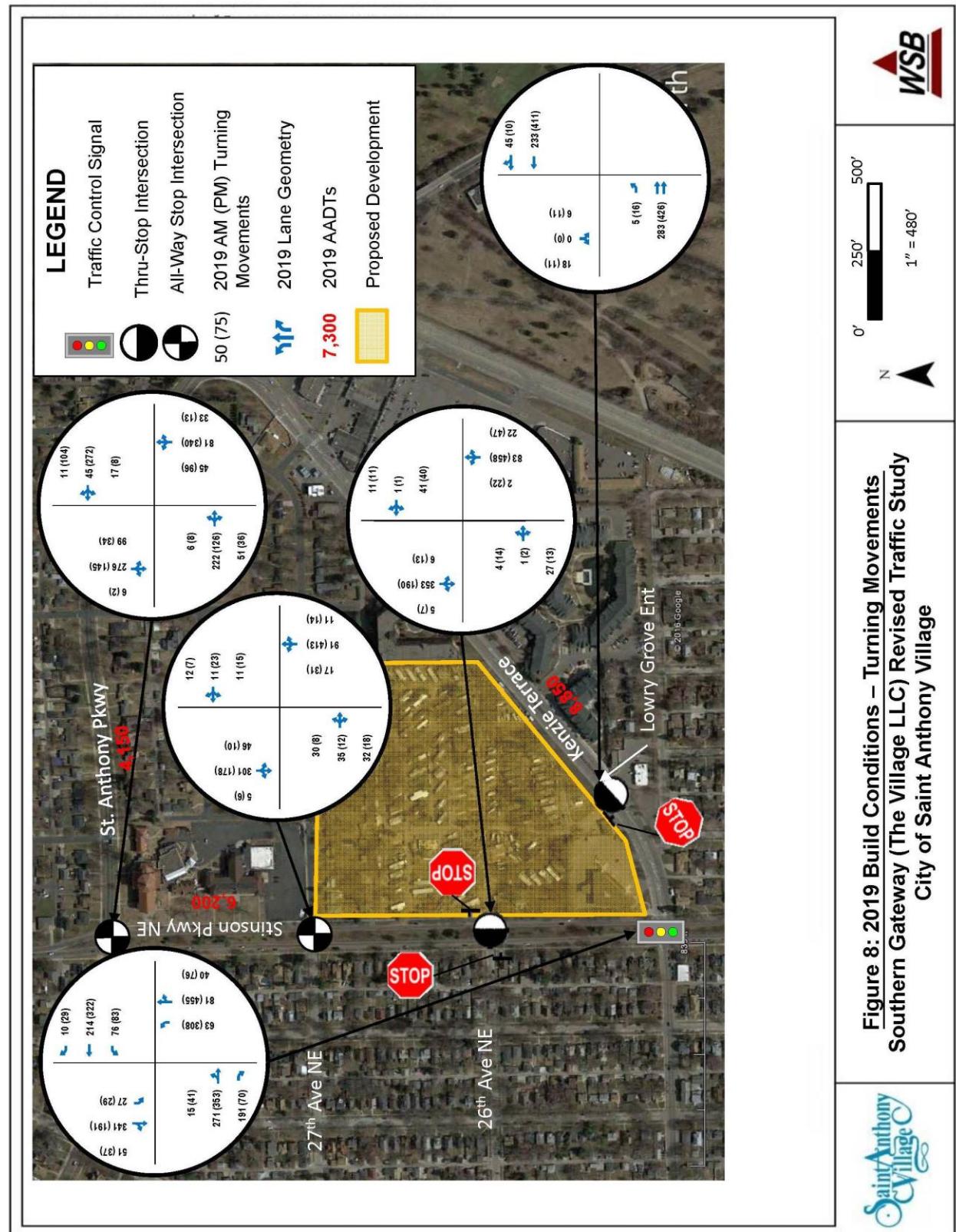
Table 4 below shows the summary of the existing and projected traffic volumes.

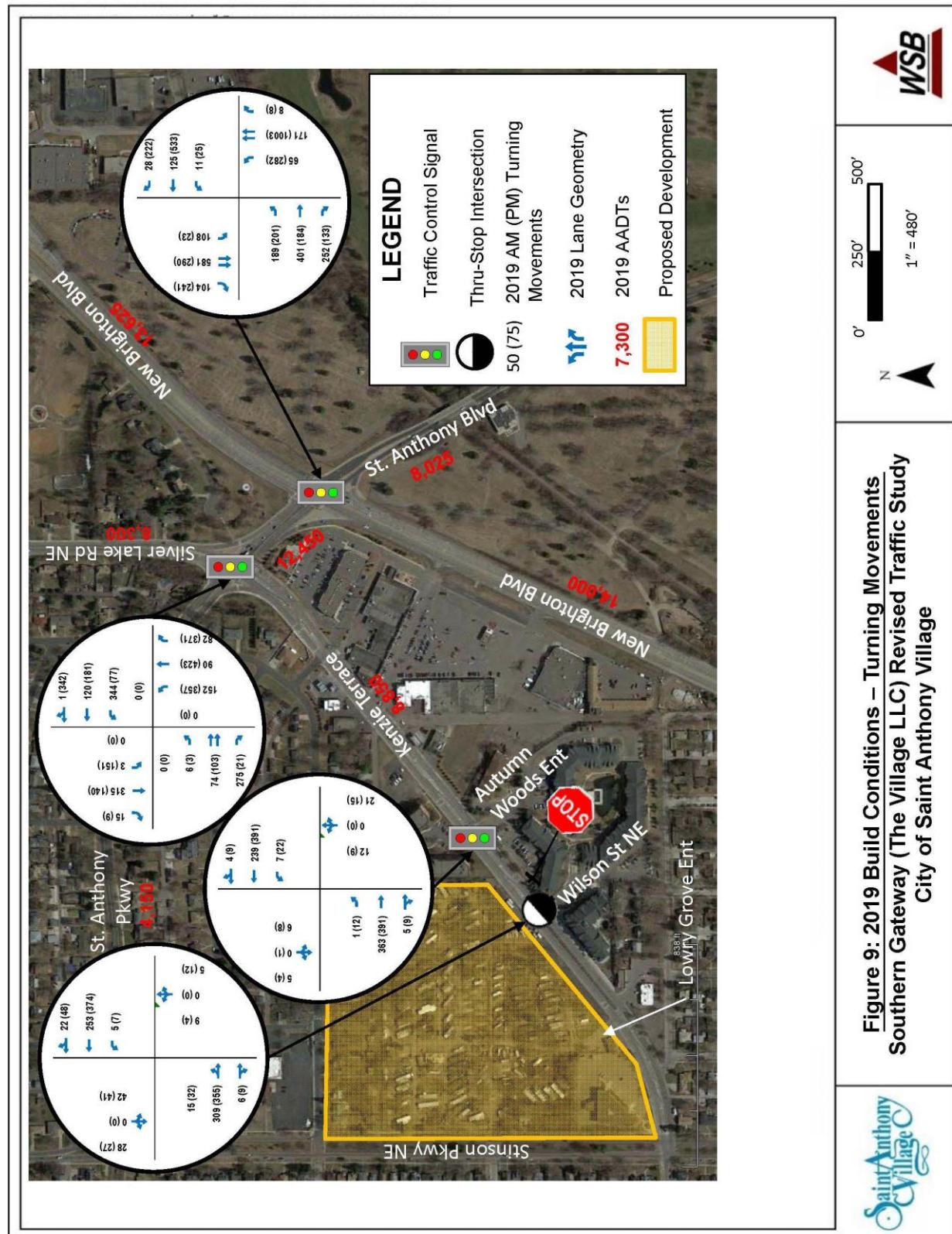
Table 4 – Traffic Volume Summary

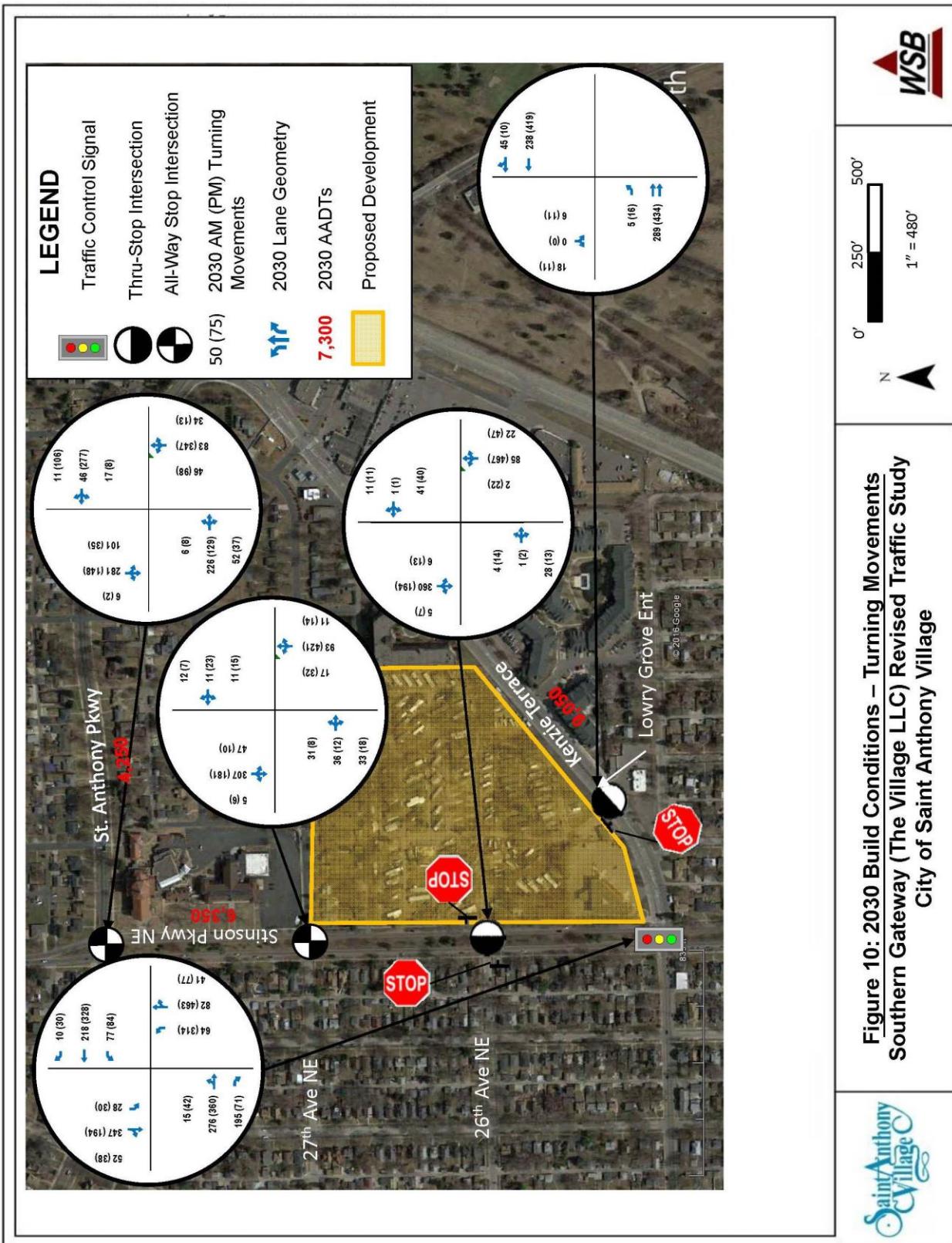
Location	Existing 2016			2030 No-Build			2030 Build			2040 ADT
	ADT	AM Peak	PM Peak	ADT	AM Peak	PM Peak	ADT	AM Peak	PM Peak	
Stinson Pkwy - South of Kenzie Tr	8,000	698	1,053	8,200	716	1,078	9,500	806	1,203	11,500
Stinson Pkwy - North of Kenzie Tr	6,000	463	697	6,150	476	734	6,350	534	797	7,250
Lowry Ave - West of Stinson Pkwy	7,000	793	1,117	7,150	812	1,142	7,300	820	1,153	8,100

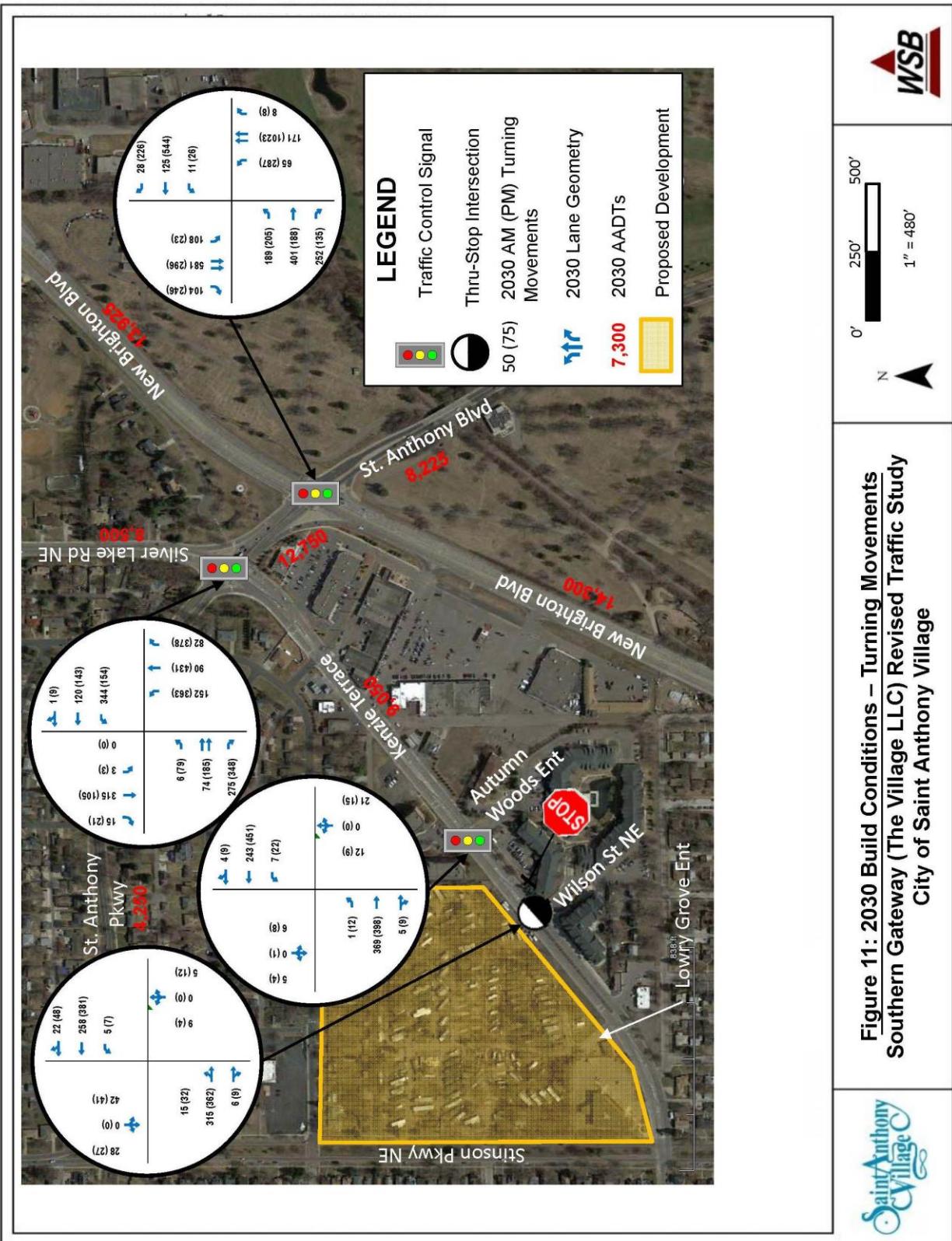












TRAFFIC IMPACT ANALYSIS

Existing and/or forecasted traffic operations were evaluated for the intersections and driveway in the study area. The analysis was conducted for the following scenarios:

1. Existing 2016
2. Projected 2019 Build
3. Projected 2030 No-Build
4. Projected 2030 Build

This section describes the methodology used to assess the operations and provides a summary of traffic operations for each scenario.

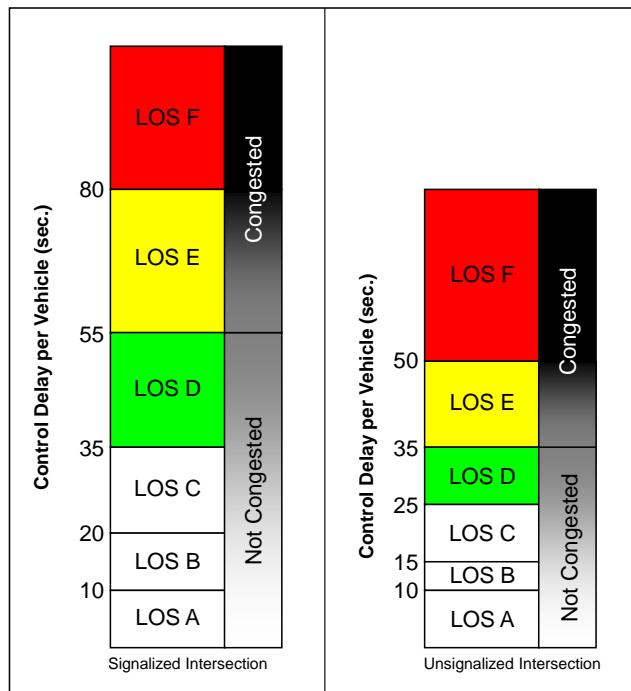
A. *Methodology*

The intersections in the corridor were evaluated during the AM and PM peak hours using Synchro/SimTraffic micro simulation software. The results are derived from established methodologies documented in the Highway Capacity Manual (HCM) 2010. The software was used to evaluate the characteristics of the roadway network including lane geometrics, turning movement volumes, traffic control and signal timing. In addition, the signal timing parameters for future year conditions were optimized using Synchro. This information was then transferred to SimTraffic (the traffic simulation model) to estimate average peak hour vehicle delays and queues.

One of the primary measures of effectiveness used to evaluate intersection traffic operations, as defined in the HCM, is Level of Service (LOS) – a qualitative letter grade, A – F, based on seconds of vehicle delay due to a traffic control device at an intersection. By definition, LOS A conditions represent high quality operations (i.e., motorists experience very little delay or interference) and LOS F conditions represent very poor operations (i.e., extreme delay or severe congestion).

Figure 12 depicts a graphical interpretation of delay times that define level of service. The delay thresholds are lower for un-signalized intersections than signalized intersections due to the public's perception of acceptable delays for different traffic controls as indicated in the HCM. In accordance with the Minnesota Department of Transportation (MnDOT) guidelines, this analysis used the LOS D/E boundary as an indicator of acceptable traffic operations.

Figure 12: Level of Service Ranges for Signalized and Un-signalized Intersections



SOURCE: Level of Service thresholds from the Highway Capacity Manual, 2000.

Existing Level of Service Summary

Table 5 - 2016 Existing Conditions shown below, summarizes the existing LOS at the primary intersections and driveways in the study area assuming the current lane geometry, traffic control and 2016 traffic volumes. The analysis results show that all intersection and driveways are operating at an overall LOS C or better during both the weekday AM and PM peak hours except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. All movements are also operating at LOS C or better, except the following which are at LOS D or worse:

- AM Peak Hour
 - NW left turn from St Anthony Blvd to New Brighton Blvd – LOS D
 - NW thru on St Anthony Blvd at New Brighton Blvd – LOS D
- PM Peak Hour
 - NB left from New Brighton Blvd to St Anthony Blvd – LOS D
 - NB thru on New Brighton Blvd at St Anthony Blvd – LOS D
 - SB left from New Brighton Blvd to St Anthony Blvd – LOS D
 - SB thru on New Brighton Blvd to St Anthony Blvd – LOS D
 - SE left from St Anthony Blvd to New Brighton Blvd – LOS D
 - NW left turn from St Anthony Blvd to New Brighton Blvd – LOS E
 - NW thru on St Anthony Blvd at New Brighton Blvd – LOS D

Table 5: 2016 Existing Conditions Level of Service Summary

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)	LOS	Overall Delay (sec/veh)
St Anthony Blvd at Stinson Pkwy	A (B)	8.9	C (C)	15.7
St Anthony Blvd at Kenzie Ter	B (C)	16.2	B (C)	14.6
St Anthony Blvd at New Brighton Blvd	B (D)	16.1	D (E)	39.9
Stinson Blvd at 27 th Street	A (A)	6.5	A (B)	8.9
Stinson Blvd at 26 th Street	A (A)	1.8	A (B)	2.4
Stinson Blvd at Lowry Ave/Kenzie Ter	B (C)	14.4	C (C)	21.3
Kenzie Ter at Lowry Grove Entrance	A (A)	0.4	A (B)	0.7
Kenzie Ter at Wilson Street	A (A)	1.1	A (A)	1.5
Kenzie Ter at Autumn Wood/Apt	B (C)	13.7	B (B)	13.9

A (A) – Overall LOS (Worst Movement LOS)

Forecasted Traffic Operations

A capacity and LOS analysis was completed for the study area intersections and driveways for 2019 which is the year after the proposed initial development would be completed and for the 2030 condition which represents the 20-year design timeframe assumed to be full development of the area. It was determined that with the minimal increase in background traffic the 2019 no-build condition would be similar to the existing 2016 conditions, therefore it was not included in the analysis. The results of the analysis are discussed below.

Table 6 – 2030 No Build Level of Service Summary, shows that all intersection and driveways are operating at an overall LOS C or better in 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. All movements are also operating at LOS C or better, however, with the increase in traffic, some movements will be operating at LOS D or worse including the following:

- AM Peak Hour
 - NW left from St Anthony Blvd to New Brighton Blvd – LOS D
 - NW thru on St Anthony Blvd at New Brighton Blvd – LOS D

- PM Peak Hour
 - NB left from New Brighton Blvd to St Anthony Blvd – LOS E
 - NB thru on New Brighton Blvd at St Anthony Blvd – LOS D
 - SB left from New Brighton Blvd to St Anthony Blvd – LOS E
 - SB thru on New Brighton Blvd at St Anthony Blvd – LOS D
 - SE left from St Anthony Blvd to New Brighton Blvd – LOS D
 - NW left from St Anthony Blvd to New Brighton Blvd – LOS F
 - NW thru on St Anthony Blvd at New Brighton Blvd – LOS F
 - NW right from St Anthony Blvd to New Brighton Blvd – LOS E

Table 6: 2030 No Build Condition Level of Service Summary

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)	LOS	Overall Delay (sec/veh)
St Anthony Blvd at Stinson Pkwy	A (B)	9.5	C (C)	15.9
St Anthony Blvd at Kenzie Ter	B (C)	18.4	B (D)	15.0
St Anthony Blvd at New Brighton Blvd	B (D)	17.7	D (F)	45.7
Stinson Blvd at 27 th Street	A (A)	6.7	A (B)	9.0
Stinson Blvd at 26 th Street	A (B)	2.9	A (C)	4.7
Stinson Blvd at Lowry Ave/Kenzie Ter	B (C)	18.2	D (E)	36.5
Kenzie Ter at Lowry Grove Entrance	A (A)	0.6	A (B)	0.8
Kenzie Ter at Wilson Street	A (B)	2.7	A (B)	3.0
Kenzie Ter at Autumn Wood/Apt	B (C)	13.8	B (C)	14.3

A (A) – Overall LOS (Worst Movement LOS)

Table 7 – Projected Build Level of Service Summary, shows that, similar to the no-build conditions, all intersection and driveways will be operating at an overall LOS C or better in 2019 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. All movements are also operating at LOS C or better, however, with the increase in traffic, some movements will be operating at LOS D or worse including the following:

- 2019
 - NB left from New Brighton Blvd to St Anthony Blvd, PM – LOS D
 - NB thru on New Brighton Blvd at St Anthony Blvd, PM – LOS D
 - SB left from New Brighton Blvd to St Anthony Blvd, PM – LOS E
 - SB thru on New Brighton Blvd at St Anthony Blvd, PM – LOS D
 - SE left from St Anthony Blvd to New Brighton Blvd, PM – LOS D
 - NW left from St Anthony Blvd to New Brighton Blvd, AM - LOS D, PM – LOS E
 - NW thru on St Anthony Blvd at New Brighton Blvd, PM – LOS E
 - NW right from St Anthony Blvd to New Brighton Blvd, PM – LOS D

- 2030
 - NB left from New Brighton Blvd to St Anthony Blvd, PM – LOS E
 - NB thru on New Brighton Blvd at St Anthony Blvd, PM – LOS D
 - SB left from New Brighton Blvd to St Anthony Blvd, PM – LOS E
 - SB thru on New Brighton Blvd at St Anthony Blvd, PM – LOS D
 - SE left from St Anthony Blvd to New Brighton Blvd, PM – LOS D
 - NW left from St Anthony Blvd to New Brighton Blvd, AM - LOS D, PM – LOS F
 - NW thru on St Anthony Blvd at New Brighton Blvd, AM – LOS D, PM – LOS F
 - NW right from St Anthony Blvd to NE Brighton Blvd, PM – LOS E

Table 7: Build Condition Level of Service Summary

Intersection	2019				2030			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Delay (sec/veh)						
St Anthony Blvd at Stinson Pkwy	A (B)	9.5	C (C)	15.7	A (B)	9.6	C (C)	16.0
St Anthony Blvd at Kenzie Ter	B (C)	17.3	B (C)	14.7	B (C)	18.5	B (D)	15.1
St Anthony Blvd at New Brighton Blvd	B (D)	16.1	D (E)	40.0	B (D)	18.1	D (F)	46.0
Stinson Blvd at 27 th Street	A (A)	6.6	A (B)	8.9	A (A)	6.8	A (B)	9.1
Stinson Blvd at 26 th Street	A (A)	2.3	A (C)	3.7	A (B)	2.9	A (C)	4.8
Stinson Blvd at Lowry Ave/Kenzie	B (C)	16.3	D (E)	32.8	B (C)	18.4	D (E)	36.9
Kenzie Ter at Lowry Grove Entrance	A (A)	0.4	A (B)	0.7	A (A)	0.6	A (B)	0.8
Kenzie Ter at Wilson Street	A (B)	1.9	A (B)	2.1	A (B)	2.8	A (B)	3.2
Kenzie Ter at Autumn Wood/Apt	B (C)	13.5	B (B)	14.0	B (C)	13.9	B (C)	14.4

A (A) – Overall LOS (Worst Movement LOS)

Vehicle Queuing Analysis

A queuing analysis for the existing and future 2019 and 2030 conditions was conducted, evaluating the anticipated vehicle queues with the build traffic conditions. The analysis was conducted using the SimTraffic simulation software. The results found that during both the weekday AM and PM peak hours in 2019 and 2030 several movements were identified that would exceed turn lane storage. These include the following movements:

- Southwest bound Silver Lake Road left turn to St Anthony Blvd will back up past existing turn storage length.
- Northeast bound Kenzie Terrace right turn to St Anthony Blvd will back up past the existing right storage length.
- Northwest bound St Anthony Blvd right turn and left turn to New Brighton Blvd will back up past the existing turn lane storage length.
- North bound Stinson Parkway left turn to Lowry Ave will back up past the existing turn lane storage length.
- West bound Lowry Avenue left turn to Stinson Parkway will back up past the existing turn lane storage length.
- East bound Kenzie Terrace right turn to Stinson Parkway will back up past the existing turn lane storage length.

CONCLUSIONS / RECOMMENDATIONS

Based on the analysis documented in this Study, WSB has concluded the following:

- The proposed Southern Gateway (revised The Village LLC) development is anticipated to consist of 414 total units (200 multi-family apartment residential units, 61 senior living units and 128 assisted living units). Access from the site will be through driveway connections to Stinson Blvd at 26th Street and to Kenzie Terrace at the existing Lowry Grove entrance.
- The proposed Southern Gateway (revised The Village LLC) development is anticipated to generate an additional 2,163 daily trips, 164 AM peak trips and 233 PM peak trips. The new Southern Gateway development generates less than half of what the previous development (The Village LLC) development was anticipated to generate.
- Existing traffic operations analysis shows that all intersection and driveways are operating at an overall LOS C or better during both the weekday AM and PM peak hours except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. There will be some movements that will be operating at LOS D or LOS E.
- Intersection traffic operations for the future 2030 no-build condition show that all intersection and driveways will operate at an overall LOS C or better during both the weekday AM and PM with the existing intersection geometrics and traffic control, except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. With the increase in traffic, some movements will be operating at LOS D or LOS E.
- Intersection traffic operations for the future build condition show that, assuming the addition of Southern Gateway development traffic, all intersection and driveways will be operating at an overall LOS C or better in 2019 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except the intersection of St Anthony Blvd at New Brighton Blvd which is operating at a LOS D during the PM Peak Hour. With the increase in traffic, some movements will be operating at LOS D, LOS E or LOS F.
- Based on the vehicle queuing analysis for the existing and future traffic conditions for weekday AM and PM peak hours several locations were identified that would exceed existing turn lane storage lengths.

- The Traffic Study evaluated condition for the years 2016 (existing), 2019 and 2030. Based on the analysis four intersections were identified with capacity or vehicle queueing impacts associated with the development of the proposed site. These intersections include:
 1. St Anthony Blvd at Kenzie Terrace/New Brighton Blvd (CR 88)
 2. St Anthony Blvd at Silver Lake Rd(CR 136)/Kenzie Terrace (CR 153)
 3. Kenzie Terrace (CR 153)/NE Lowry Ave at NE Stinson Parkway
 4. Kenzie Terrace (CR 153) at Proposed Site Access/Wilson Street

Based on the analysis mitigation improvements are recommended for 2019 with the proposed development and by 2030 as the area continues to develop. These recommendations include:

2019 with Southern Gateway Development:

1. Request Hennepin County to optimize the signal time and coordination between the St Anthony Blvd intersections at New Brighton Blvd (CR 88) and Silver Lake Rd (CR 136)/Kenzie Terrance (CR 153).
2. Lengthen the northwest bound left turn lane from St Anthony Blvd to southbound New Brighton Blvd (CR 88) from 125 feet to 225 feet.
3. Lengthen the southwest bound left turn from Silver Lake Rd (CR 136) to southeast St Anthony Blvd from 100 feet to 175 feet.
4. Lengthen the westbound left turn from Kenzie Terrace (CR 153) to southbound NE Stinson Parkway when the Bremer Bank Building site is redeveloped.
5. Lengthen the northbound left turn from NE Stinson Parkway to westbound NE Lowry Ave from 150 feet to 250 feet.
6. At the proposed site driveway at Wilson St on Kenzie Terrace (CR 153) provide:
 - a. Two lanes exiting the site (one left turn and one through/right lane)
 - b. Left turn lane from Kenzie Terrace (CR 153) into the site
 - c. Right turn lane from Kenzie Terrace (CR 153) into the site

2030 with Future Area Development:

1. Consider a dual lane roundabout or other traffic control improvements for the St Anthony Blvd intersections at New Brighton Blvd (CR 88) and Silver Lake Rd (CR 136)/Kenzie Terrance (CR 153).
2. Consider a roundabout or other traffic control improvements at the intersection of Kenzie Terrace (CR 153)/NE Lowry Ave at NE Stinson Parkway.



The Village LLC Development - Parking Analysis
1.25.2018

PARKING DEMAND PER CITY ORDINANCE				
	Total Unit Count	Stalls Required/Unit	Total Parking Demand	Comment
Independent (IL) Senior	61	2	122	
Assisted (AL/MC) Senior	128	1/3	63	includes 20 staff
Market Rate (MR)	225	2	450	

635

PARKING DEMAND ANALYSIS BY BEDROOM QUANTITY						
	1/3	1/3	1/3	1/3	1/3	1/3
	1	1	1	1	2	3
	IL/MC ratios	IL/MK/AF ratios				
Studio						
IL	0	0	16	14	24	54
AL/MC	9	0	18	12	5	0
MR	0	79	98	0	72	36
						285

456

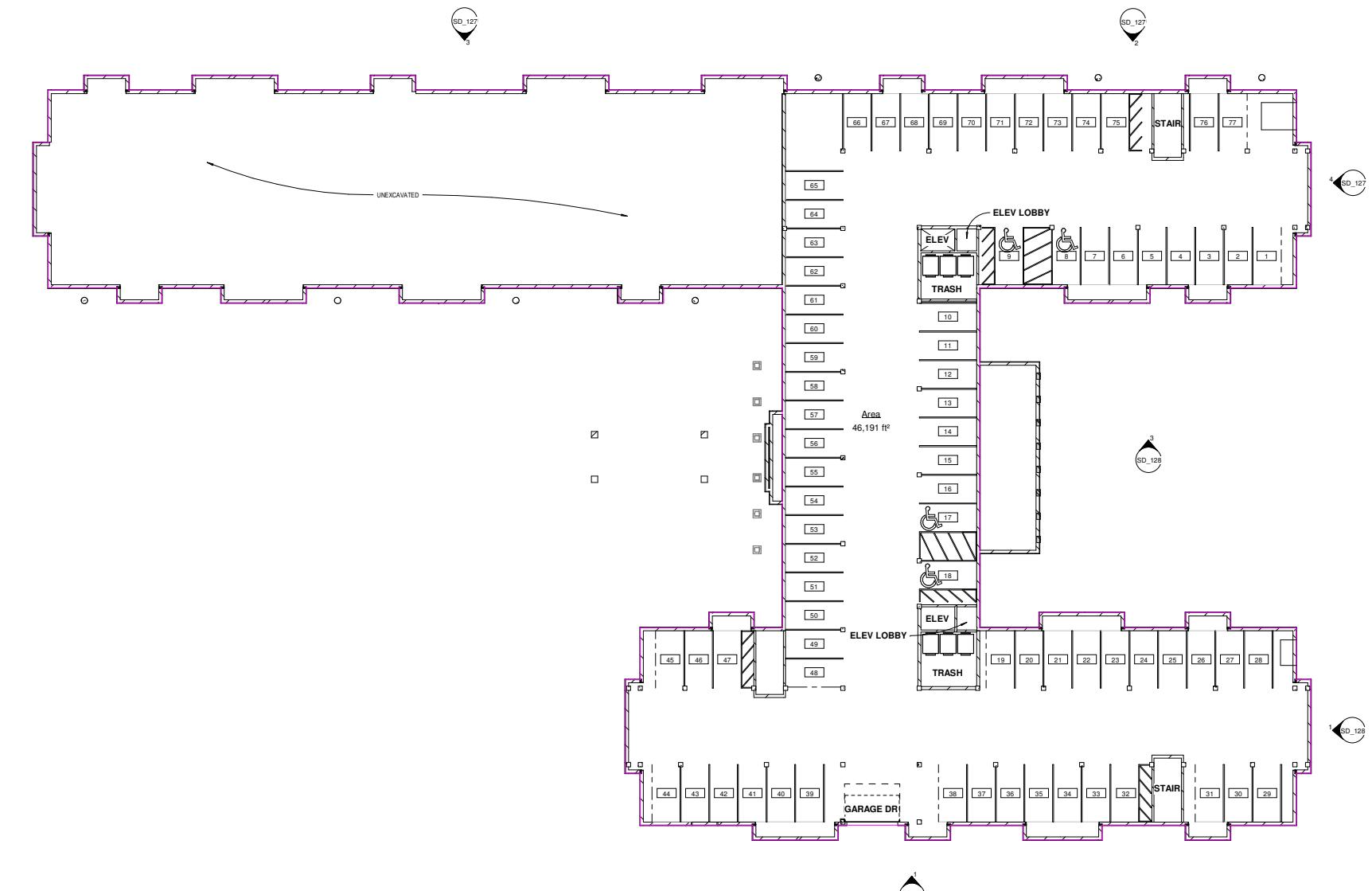
PARKING AVAILABILITY					
	Surface Parking	Garage Parking	Total Parking	Avail.-Code Demand	Comment
Independent (IL) Senior	34	92	126	4	
Assisted (AL/MC) Senior	34	77	111	48	
Market Rate (MR)	218	238	456	6	Includes proof of parking on west side

693

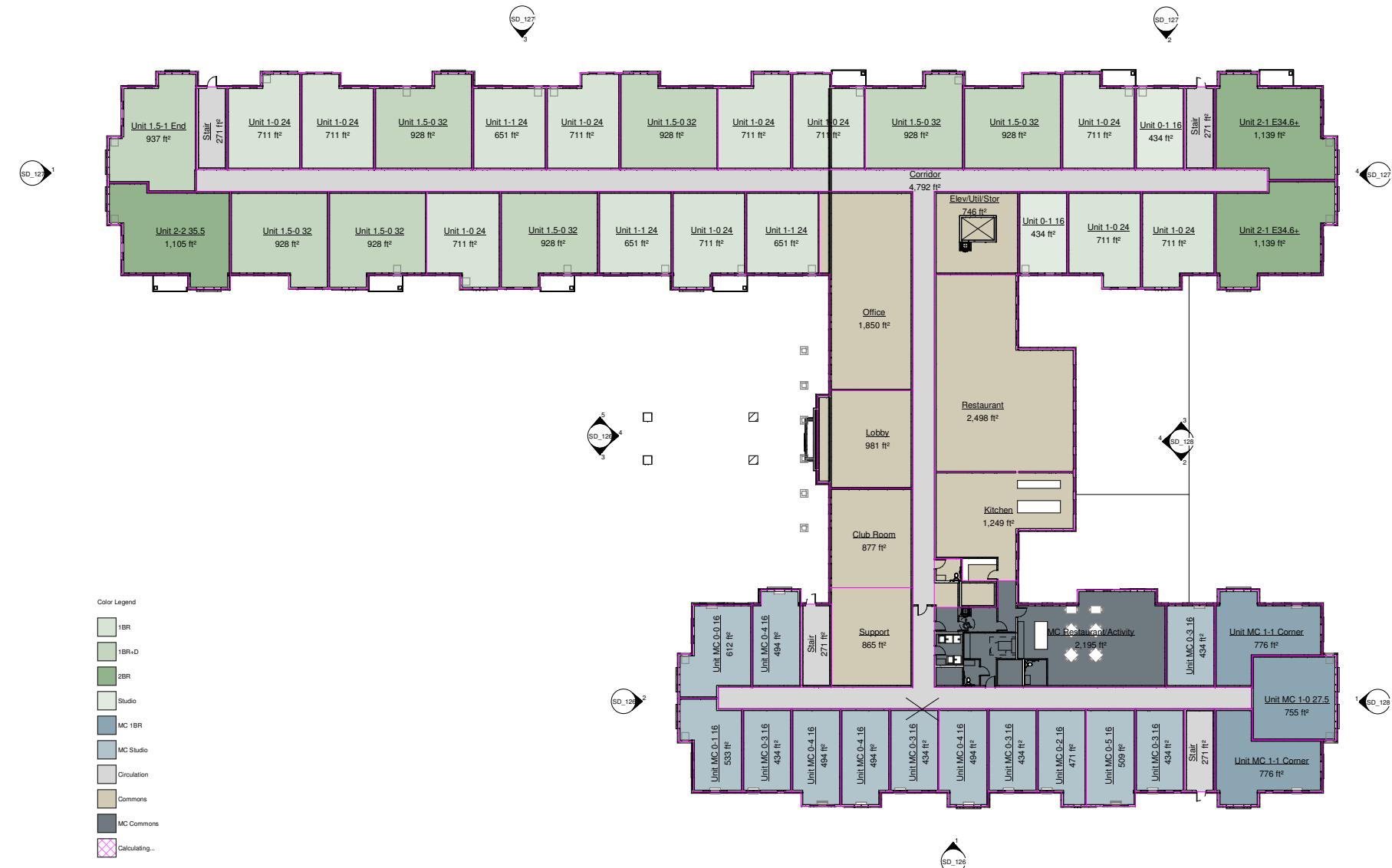
Residential Unit Mix		
Name	Count	Gross Area
17059-Continental-St.Anthony-Assisted Senior		
Unit 0-1 16	11	434 ft ²
Unit 1-0 24	34	711 ft ²
Unit 1-1 24	13	651 ft ²
Unit 1-2 24	2	650 ft ²
Unit 1-3 24	2	762 ft ²
Unit 1.5-0 32	29	928 ft ²
Unit 1.5-1 End	3	937 ft ²
Unit 1.5-1 End-2	2	886 ft ²
Unit 1.5-2 32	1	868 ft ²
Unit 2-1 E34.6+	10	1,139 ft ²
Unit 2-2 35.5	5	1,105 ft ²
Unit MC 0-0 16	1	612 ft ²
Unit MC 0-1 16	1	533 ft ²
Unit MC 0-2 16	1	471 ft ²
Unit MC 0-3 16	5	434 ft ²
Unit MC 0-4 16	4	494 ft ²
Unit MC 0-5 16	1	509 ft ²
Unit MC 1-0 27.5	1	755 ft ²
Unit MC 1-1 Corner	2	776 ft ²
17059-Continental-St.Anthony-Assisted Senior: 128	128	

Residential Unit Mix		
Name	Count	Gross Area
17059-Continental-St.Anthony-Independent Senior		
Unit 1-0 25.5	9	704 ft ²
Unit 1-1 25.5	7	795 ft ²
Unit 1.5-0	3	1,128 ft ²
Unit 1.5-1	1	1,134 ft ²
Unit 1.5-2	2	1,048 ft ²
Unit 1.5-3	6	1,071 ft ²
Unit 1.5-4	3	1,179 ft ²
Unit 2-0	3	1,406 ft ²
Unit 2-1	6	1,259 ft ²
Unit 2-2	3	1,364 ft ²
Unit 2.5-0	12	1,606 ft ²
Unit 2.5-1	6	1,432 ft ²
17059-Continental-St.Anthony-Independent Senior: 61	61	

Residential Unit Mix		
Name	Count	Gross Area
17059-Continental-St.Anthony-Market Rate		
Unit 0-1	8	639 ft ²
Unit 0-2	8	619 ft ²
Unit 0-3	61	616 ft ²
Unit 1-1	60	740 ft ²
Unit 1-2	40	824 ft ²
Unit 2-1	12	1,113 ft ²
Unit 2-3	8	1,369 ft ²
Unit 2-5	4	1,357 ft ²
Unit 2-6	4	1,085 ft ²
Unit 2-7	8	1,206 ft ²
Unit 3-1	8	1,382 ft ²
Unit 3-2	4	1,282 ft ²
17059-Continental-St.Anthony-Market Rate: 225	225	
Grand total: 414	414	



① Level -1
1/16" = 1'-0"



Assisted Senior First Floor Plan
The Village, LLC







① South Elevation
1/8" = 1'-0"



② Southwest End Bay Elevation
1/8" = 1'-0"



③ Southwest Elevation
1/8" = 1'-0"



④ Main Entry West Elevation
1/8" = 1'-0"



⑤ Northwest Elevation
1/8" = 1'-0"

EXTERIOR MATERIALS KEY	
① CMU	
② Brick	
③ Block Soldier Course	
④ Stone Sill	
⑤ Aluminum Deck Rail	
⑥ CFB Lap Siding, Color 1	
⑦ CFB Shake Siding, Color 1	
⑧ CFB Panel, Color 3	
⑨ Prefabricated PTAC Grate, Color TBD	
⑩ Prefabricated Metal Magic Pak Grate, Color TBD	

Assisted Senior Exterior Elevations
The Village, LLC



① Northwest End Bay Elevation
1/8" = 1'-0"

③ North Elevation
1/8" = 1'-0"



② North Elevation - 1
1/8" = 1'-0"

④ Northeast End Bay Elevation
1/8" = 1'-0"

EXTERIOR MATERIALS KEY

- (1) CMU
- (2) Brick
- (3) Brick Soldier Course
- (4) Stone Wall
- (5) Aluminum Deck Rail
- (6) CFB Lap Siding, Color 1
- (7) CFB Shake Siding, Color 2
- (8) CFB Panel, Color 3
- (9) CFB Trim, Color 3
- (10) Prefinished Metal Manic Park Grille, Color TBD
- (11) Prefinished Metal Manic Park Grille, Color TBD



③ Northeast Elevation
1/8" = 1'-0"



④ East Elevation
1/8" = 1'-0"

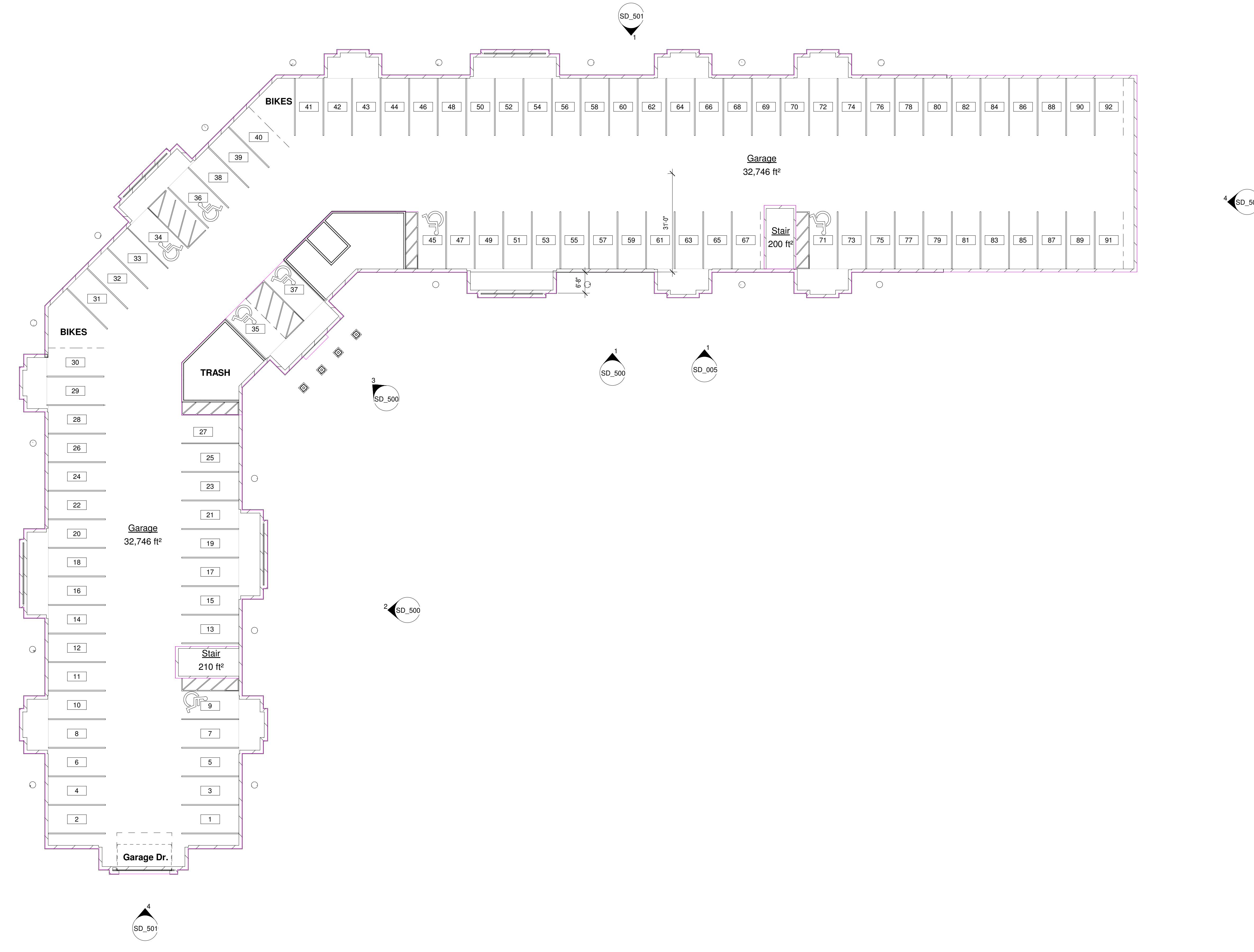


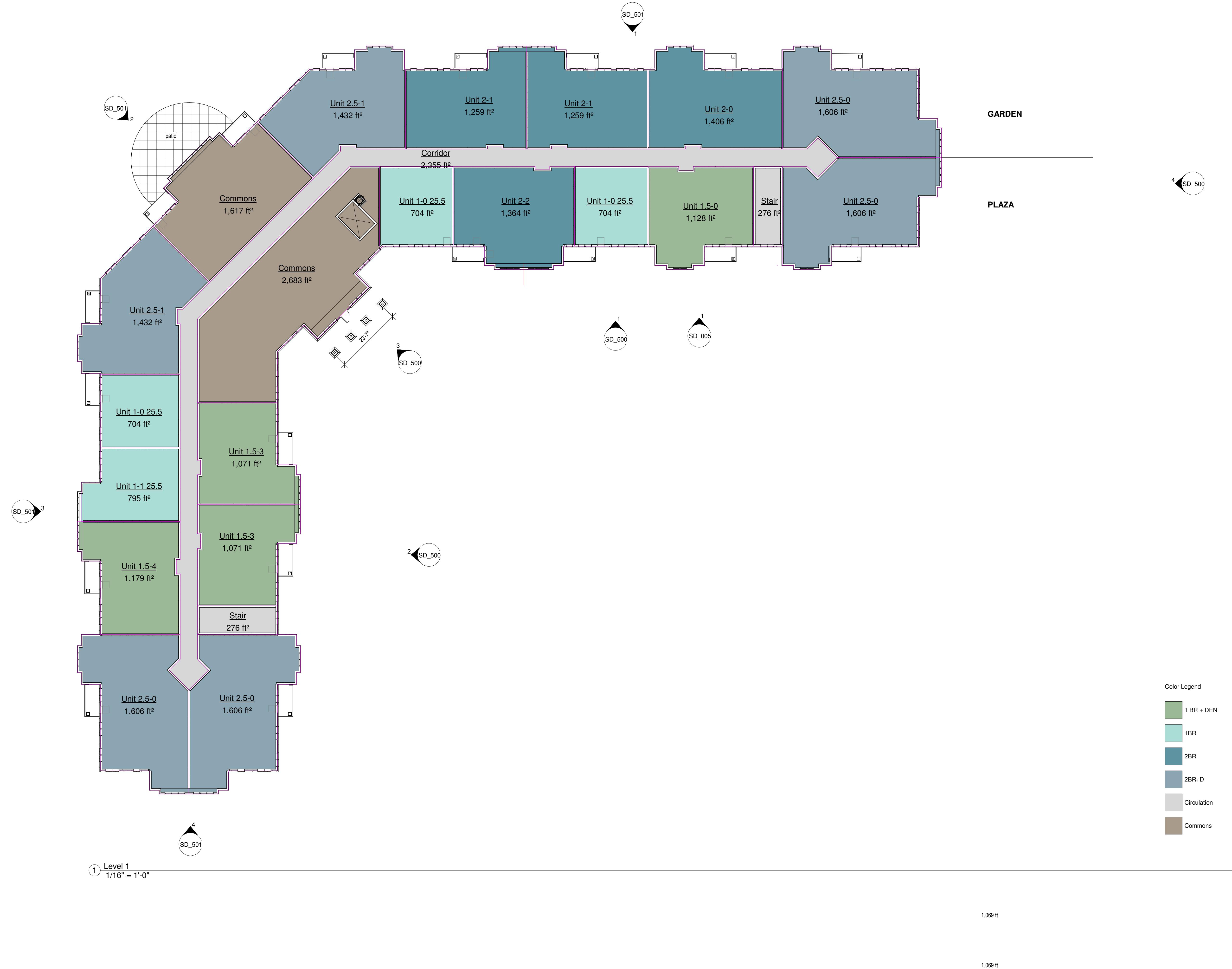
② Southeast Elevation
1/8" = 1'-0"



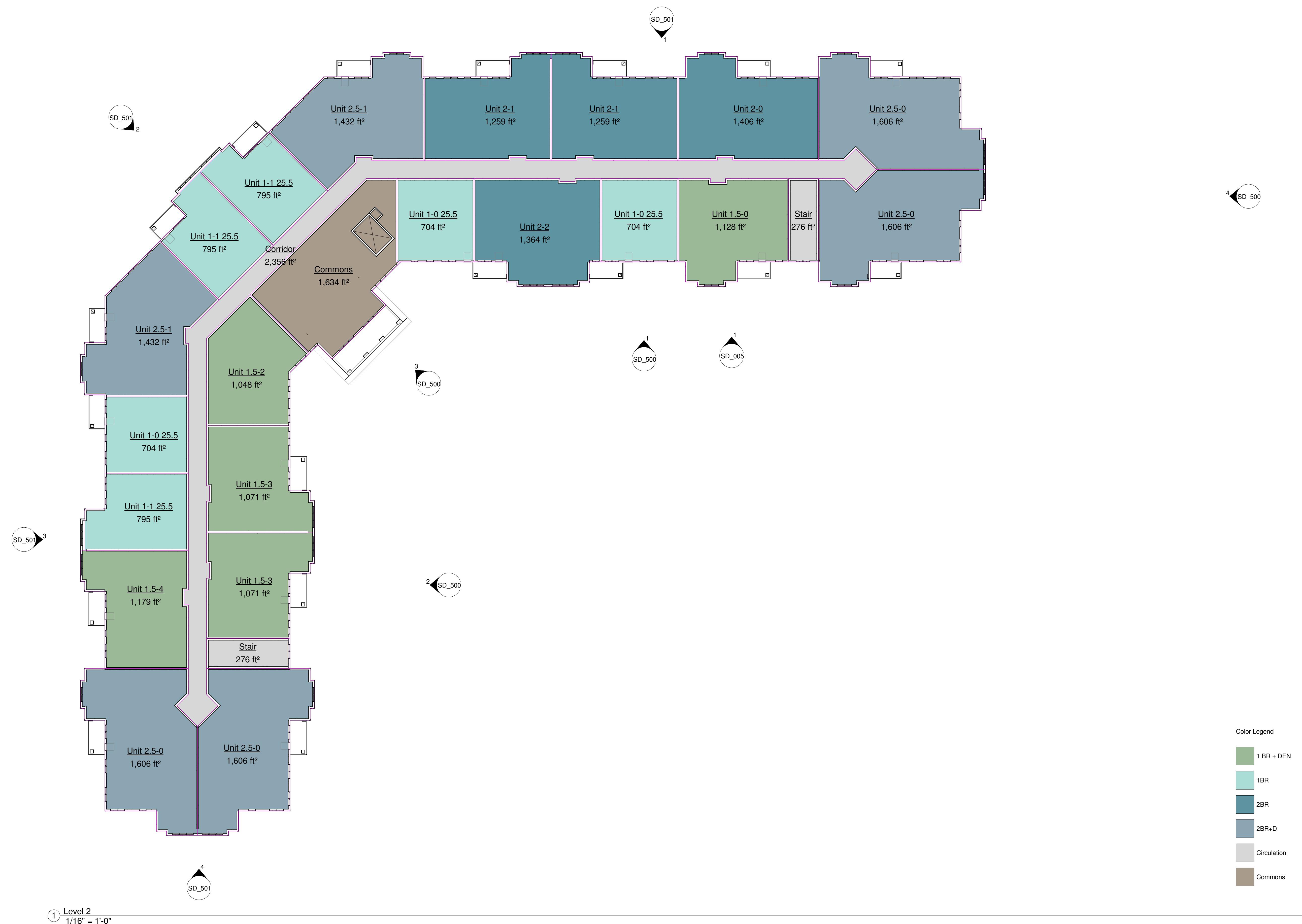
① Southeast End Bay Elevation
1/8" = 1'-0"

EXTERIOR MATERIALS KEY	
① CMU	Brick
② Brick Soldier Course	Stone Sill
③ Aluminum Deck Rail	CFB Lap Siding, Color 1
④ CFB Lap Siding, Color 2	CFB Panel, Color 2
⑤ CFB Trim, Color 3	CFB Trim, Color 3
⑥ Prefinished PTAC Grille, Color TBD	Prefinished Metal Magic Pak Grille, Color TBD
⑦ Prefinished Metal Magic Pak Grille	



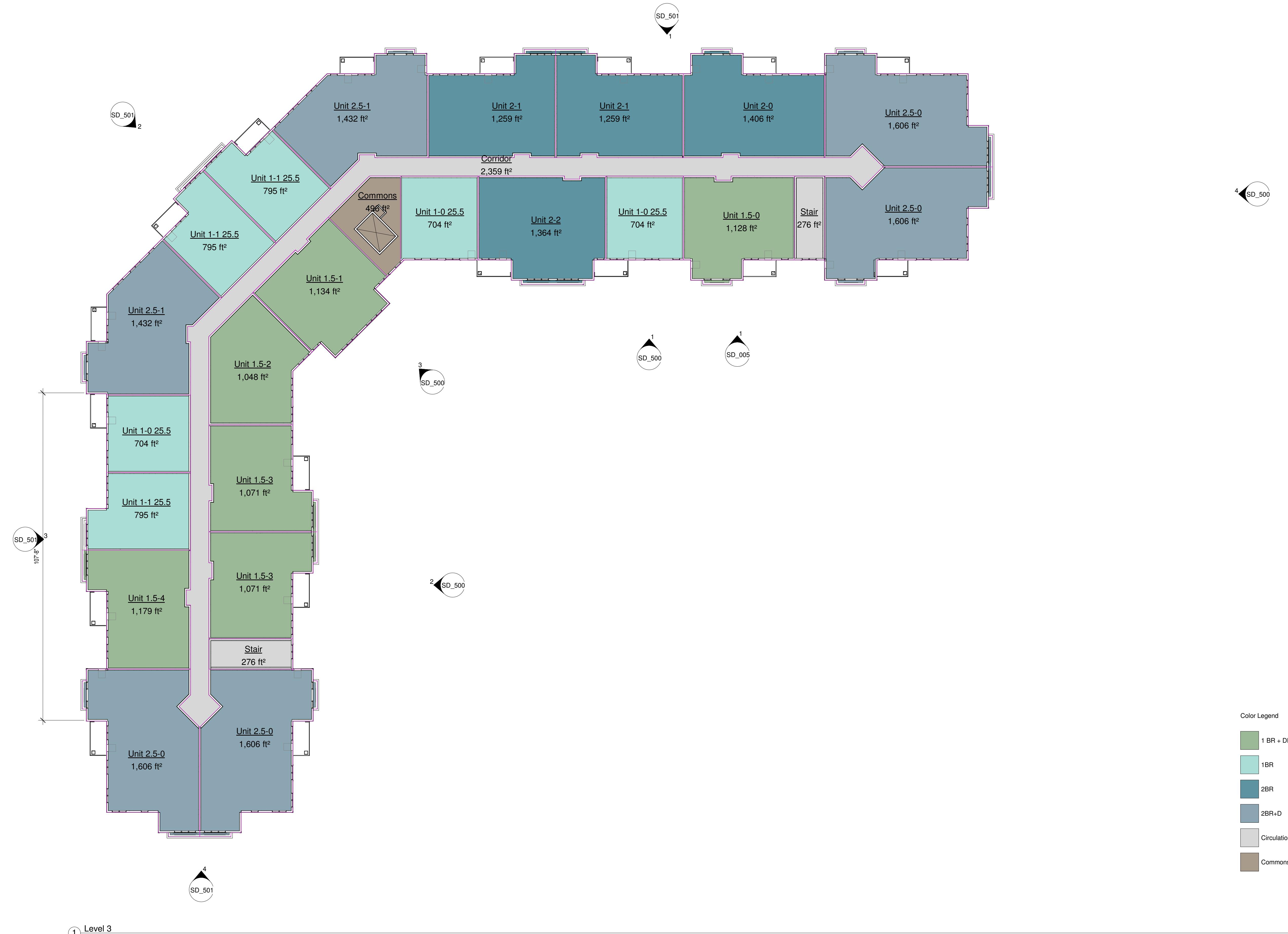


Independent Senior First Floor Plan
The Village, LLC



Independent Senior Second Floor Plan

The Village, LLC

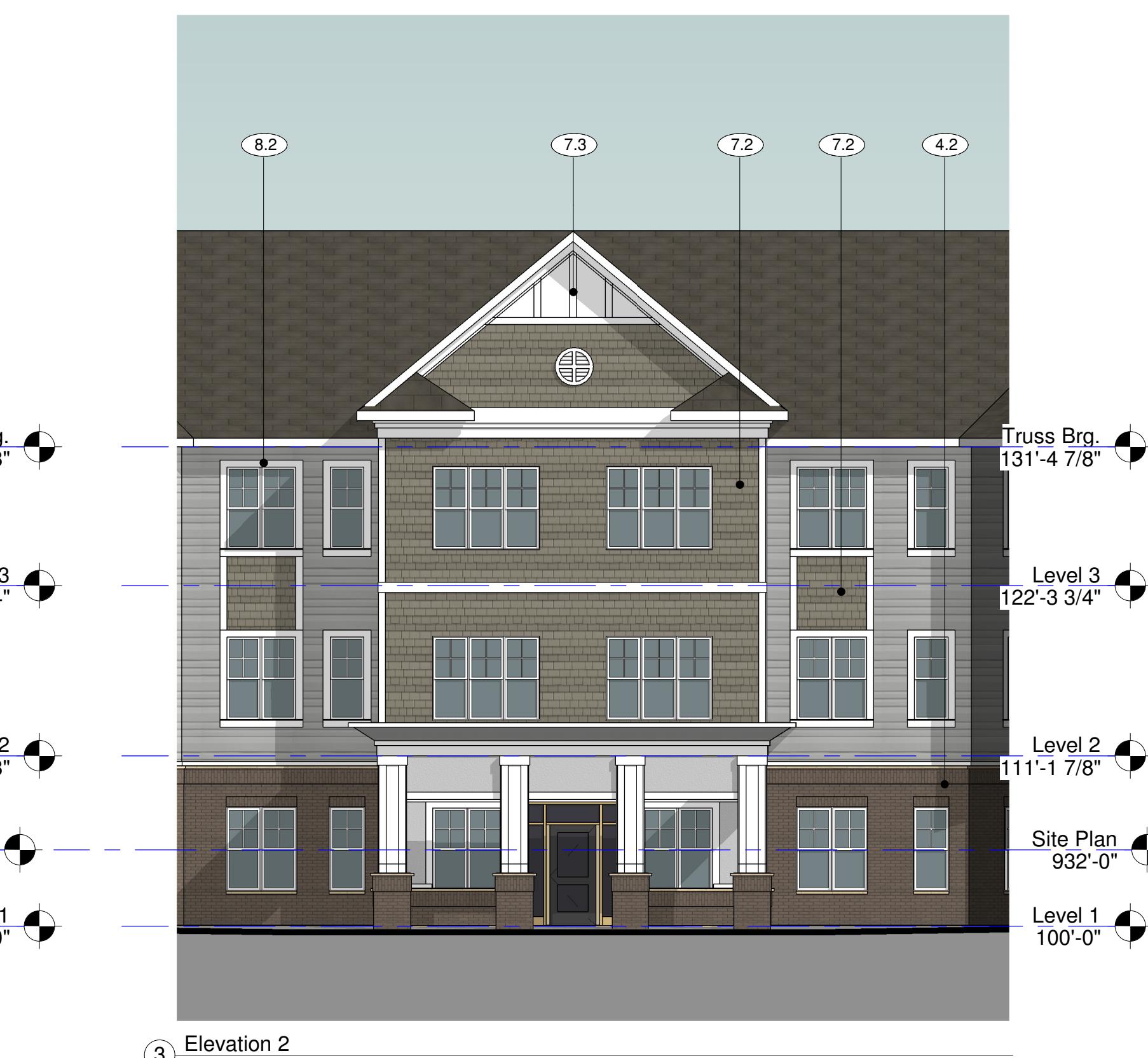


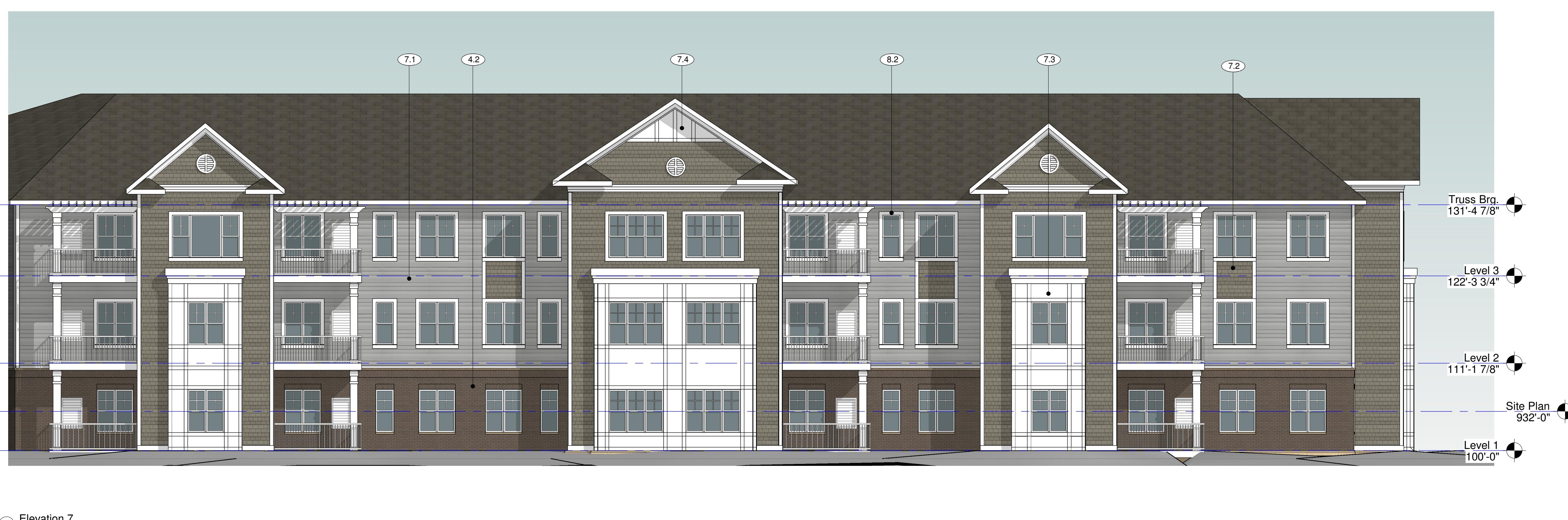
Independent Senior Third Floor Plan

The Village, LLC

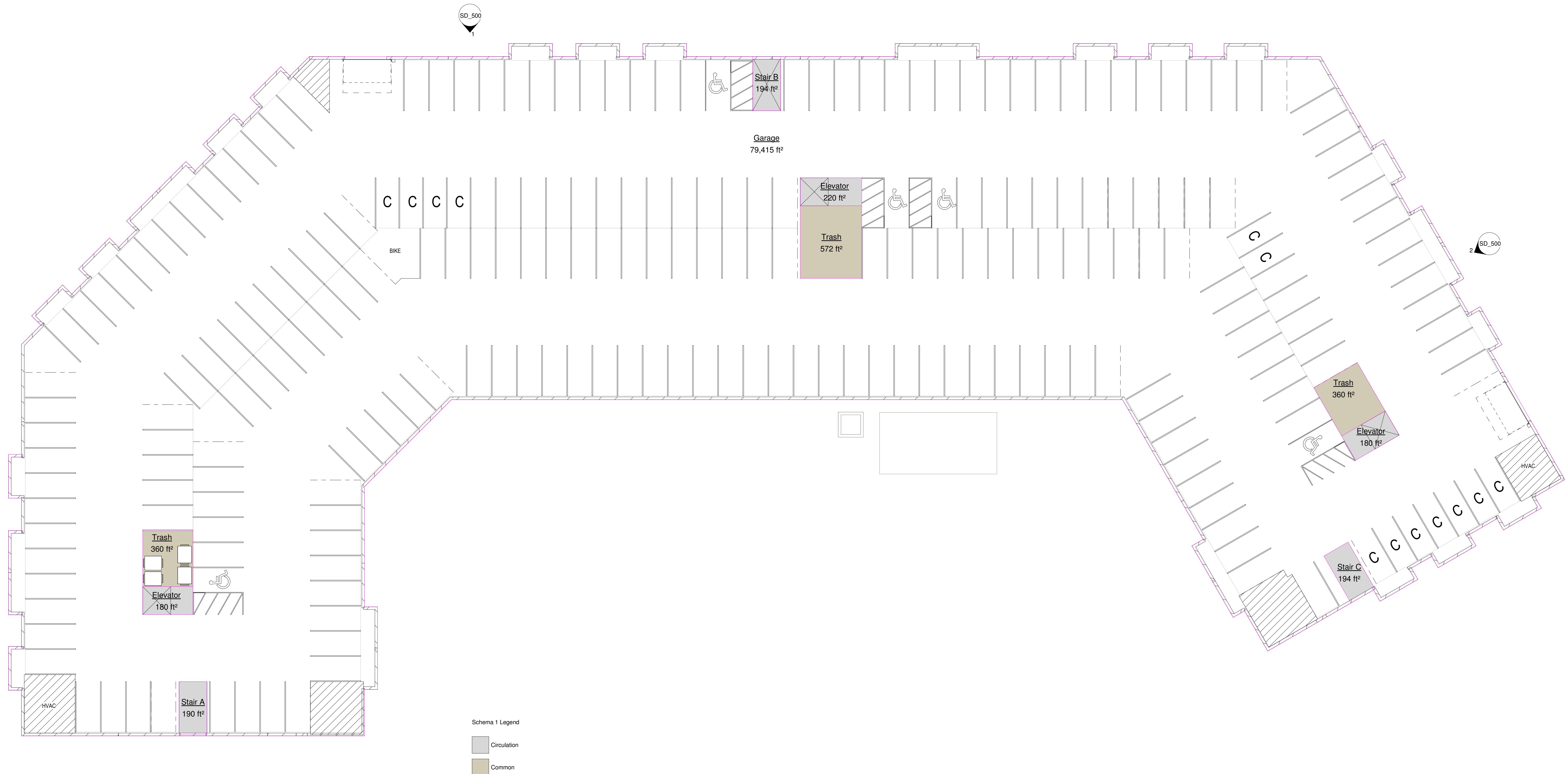


EXTERIOR MATERIALS KEY



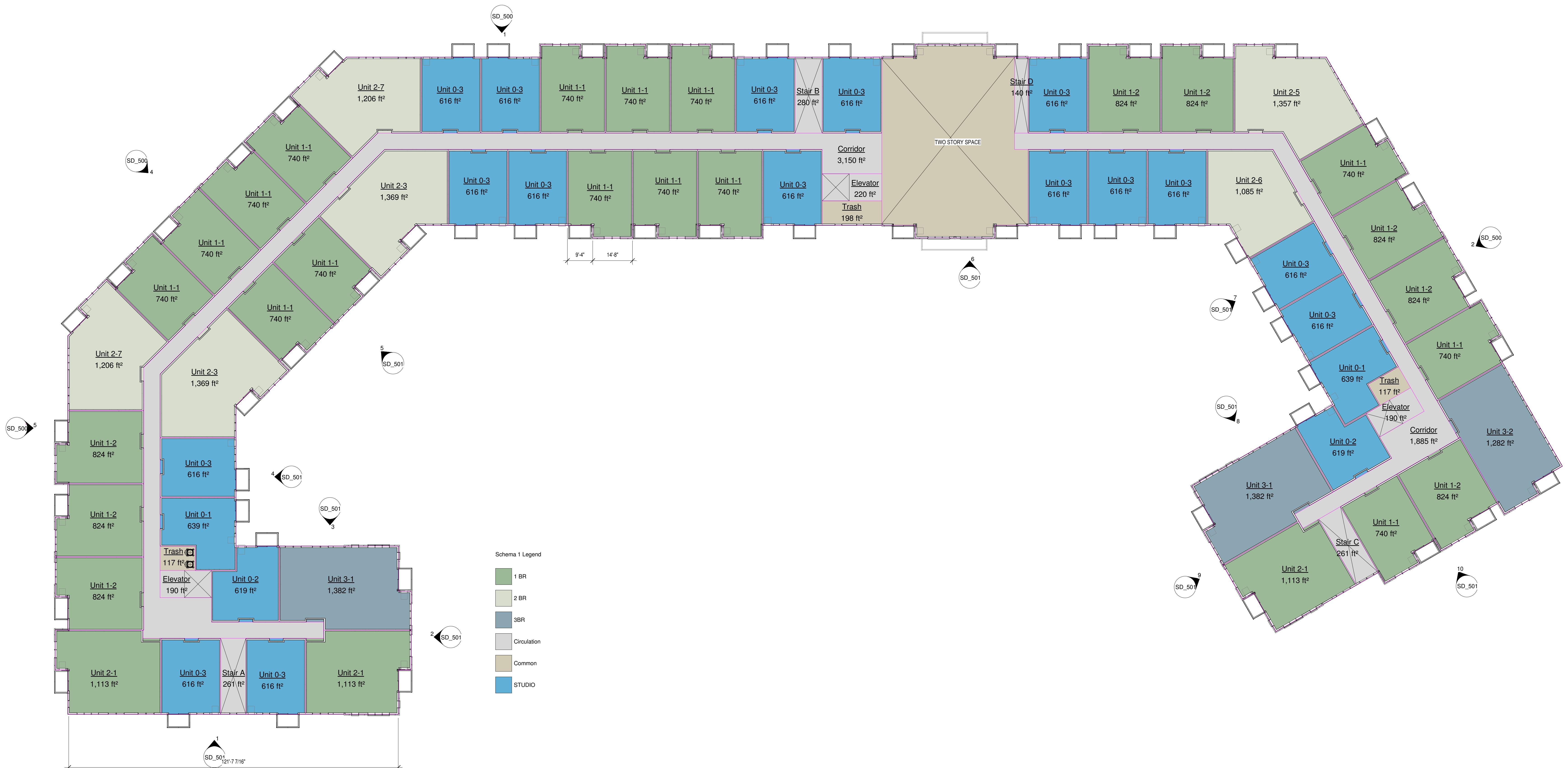


Independent Senior Exterior Elevations
The Village, LLC



① Level -1
1/16" = 1'-0"

Market Rate Building Garage Floor Plan
The Village, LLC

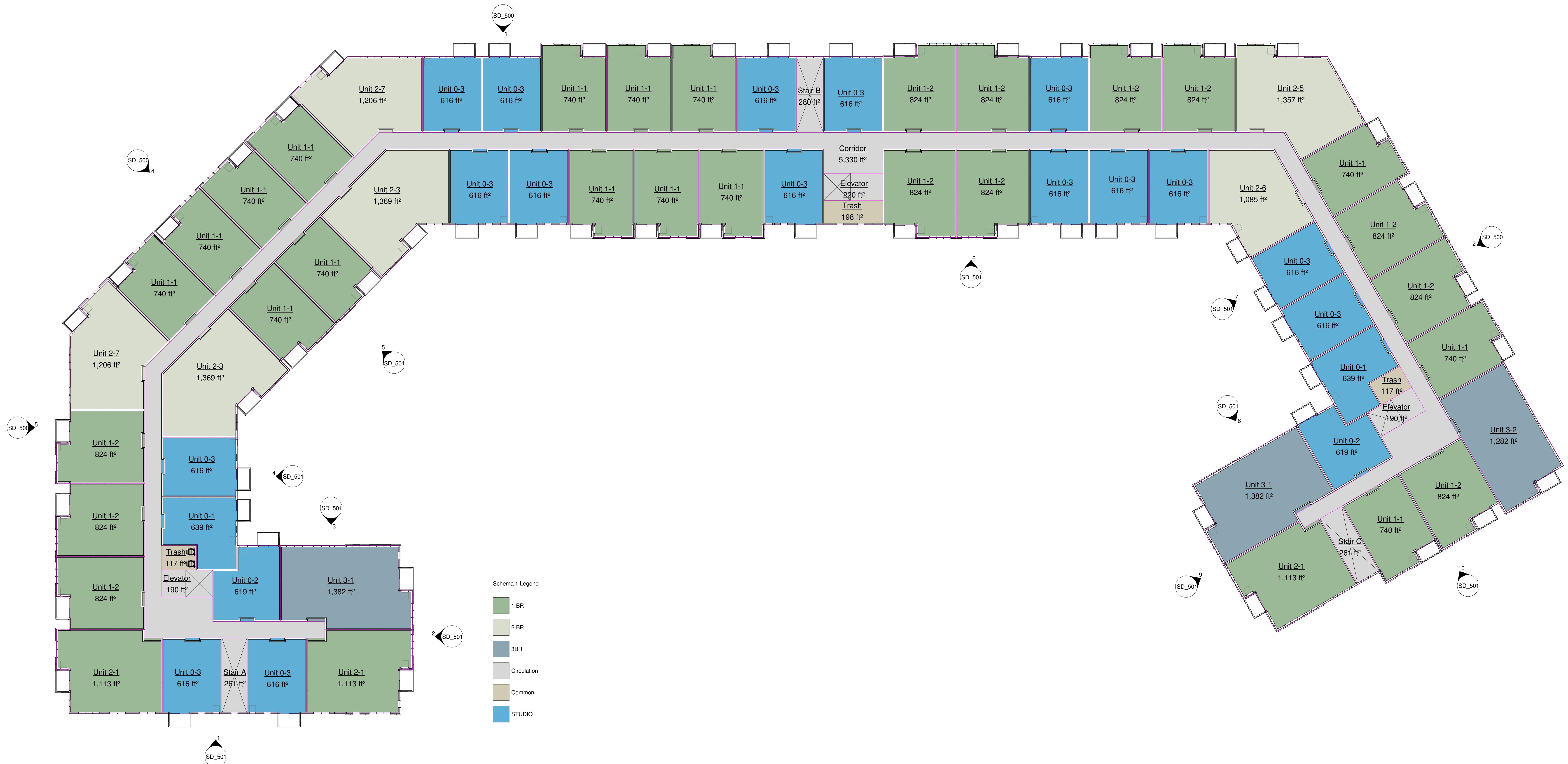


1 Level 2
 $1/16" \equiv 1'-0"$

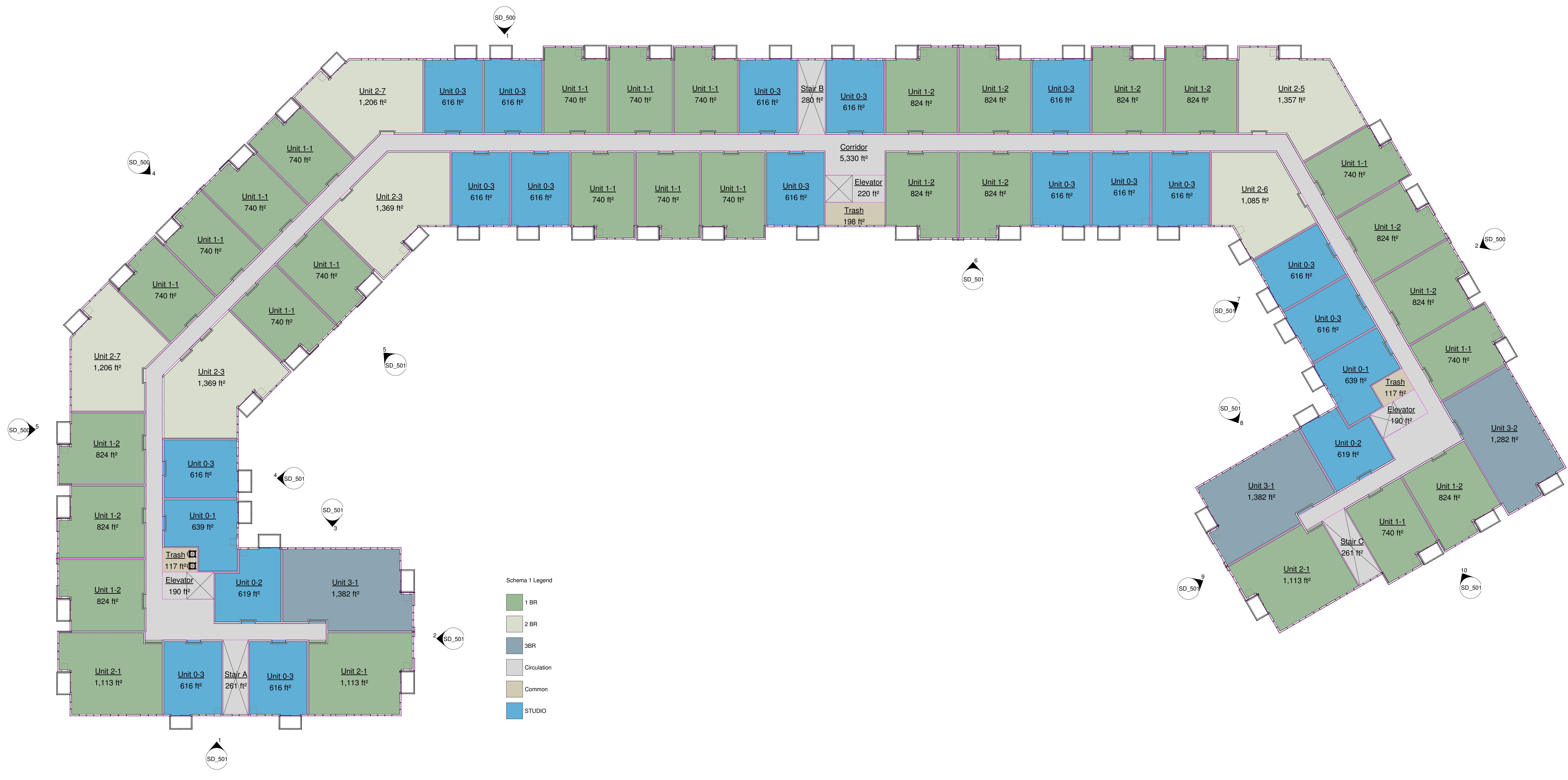
1 Level 2
1/16" = 1'-0"

Market Rate Second Floor Plan

The Village, LLC



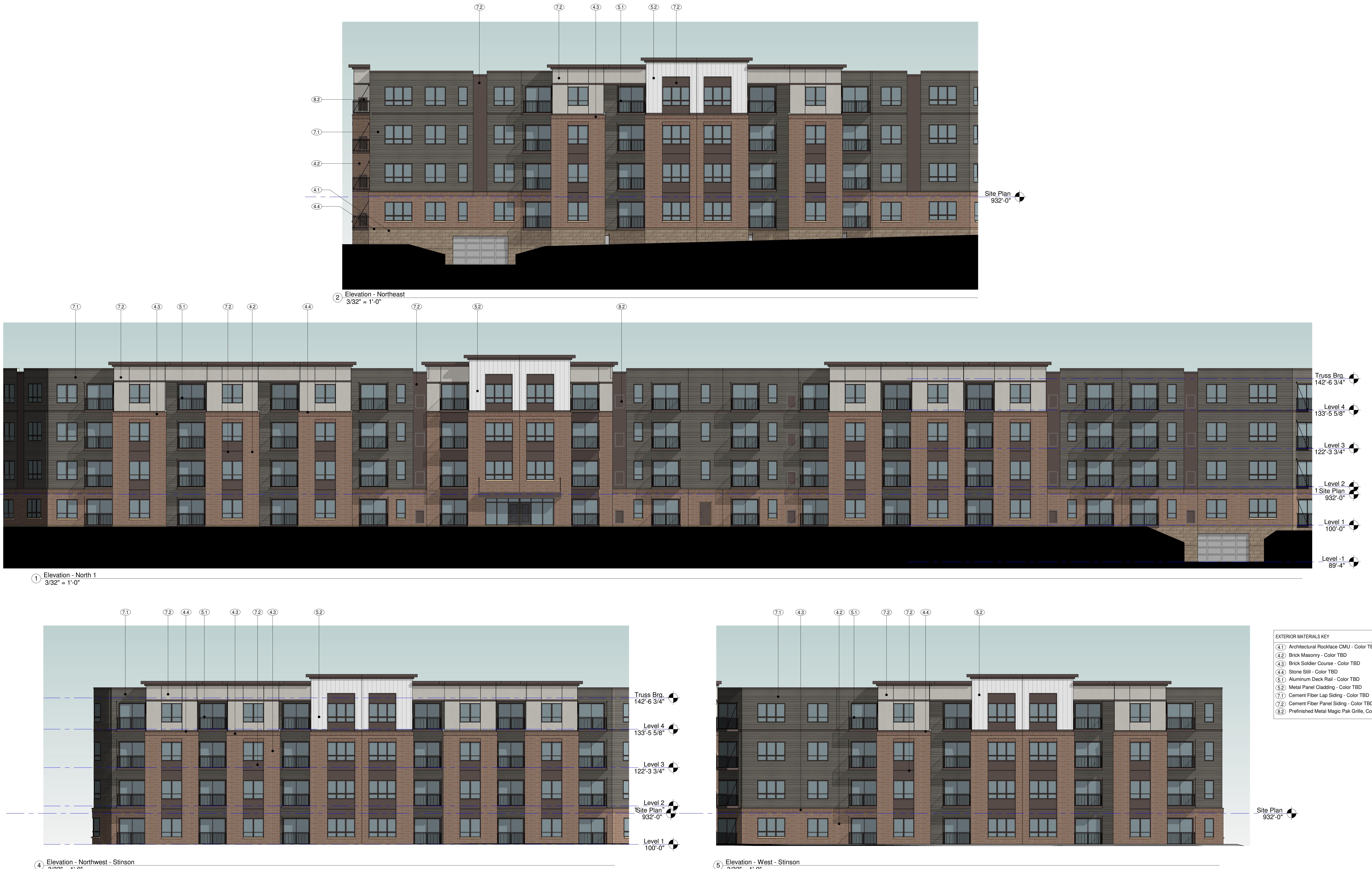
1 Level 3
1/16" = 1'-0"



1 Level 4
1/16" = 1'-0"

Market Rate Fourth Floor Plan

The Village, LLC



Market Rate Building Exterior Elevations
The Village, LLC



EXTERIOR MATERIALS KEY	
4.1	Architectural Rockface CMU - Color TBD
4.2	Brick Masonry - Color TBD
4.3	Brick Soldier Course - Color TBD
4.4	Stone Still - Color TBD
5.1	Aluminum Deck Rail - Color TBD
5.2	Metal Panel Cladding - Color TBD
7.1	Cement Fiber Lap Siding - Color TBD
7.2	Cement Fiber Panel Siding - Color TBD
8.2	Prefinished Metal Magic Pak Grille, Color TBD



February 13, 2018

Ms. Traci Tomas
Continental Property Group

Re: The Village LLC Development
St. Anthony Village, MN
Preliminary PUD Plan Review
WSB Project No. 02170-300

Dear Ms. Tomas:

We have reviewed the development plan submittal received on January 29, 2018, for the above referenced project. Plans have been prepared by Wenck Associates, dated January 26, 2017 and include the storm water management plan, preliminary plat, site plan, grading plan, sanitary sewer, water, and storm utility plans.

We have the following comments and recommendations for this preliminary PUD Plan review.

General

1. Prior to the start of any construction, permits will need to be obtained and submitted to the city from the following agencies:
 - a. MPCA Construction General Permit
 - b. MPCA Sanitary Sewer Extension Permit
 - c. MnDOH Watermain Extension Permit
2. Prior to the start of any construction, project approval will need to be obtained and submitted to the city from the following agencies:
 - a. Hennepin County (access and traffic control associated with Kenzie Terrace/ CR 153)
 - b. Minneapolis Park Board (access and traffic control associated with Stinson Parkway)
3. An operations and maintenance component will be required to ensure that the stormwater treatment facilities will be maintained long term. The declarations must at a minimum include the following:
 - a. Annual inspection and reporting to the city
 - b. Maintenance of all erosion control measures including, but not limited to: rip rap, storm sewer outlets, catch basin inlets, etc. (annual documentation required)
 - c. Verification of system drawdowns with 48 hours (annual documentation required)
 - d. Removal of sediment, trash and debris (annual documentation required)

Cover Sheet and Project Notes

1. Reference the most recent edition (2018) of the City of St. Anthony's General Specifications and Standard Detail Plates for Street and Utility Construction as a governing specification.

Preliminary Plat

1. Remove the erroneous line within the 15' drainage and utility easement along Kenzie Terrace.

2. Simplify the drainage and utility easements around the ponds and along the west side of the development by squaring them off and combining them with nearby and adjacent drainage and utility easements.
3. Provide an easement for the shared hydrant service lead crossing over between both lots 1 and 2 of block 1.

Existing Conditions, Erosion, and Sediment Control

1. Submit erosion control plans with final plat/construction plan submittal.

Site Plan

1. Provide 6' wide concrete walks per City standards where 5' concrete walks are currently shown.
2. Show necessary removals for the installation of the proposed utilities along Kenzie Terrace/ CR 153.

Grading Plan

1. Structural calculations for the proposed retaining walls greater than 4.0' in height need to be submitted for review prior to construction of the walls. The submittal will need to include:
 - a. Certification of a qualified MN licensed professional
 - b. Soil boring logs and geotechnical recommendation for support
 - c. Identify the type of material for the wall and design details
 - d. Provide details of the wall construction
 - e. Provide a safety railing or fence above the wall
2. Driveway ramps into the buildings should not exceed 10%.
3. The outdoor patios and plazas with the buildings do not appear to have any slope. How will surface drainage be accomplished?
4. The NWL of the southwestern stormwater pond is the same elevation as the Multi-residential 1 garage floor (912.0).
5. The HWL of both ponds exceed the garage levels of the connected buildings.
6. Demonstrate how the proposed improvements accommodate for the current ground water elevations.
7. There are areas where the slopes shown do not meet the minimum required 2% or exceed the maximum allowed 3:1.
8. The driveway elevations at the top of the ramps need to be raised to achieve a minimum 1' freeboard above the adjacent road's EOF.

Sanitary Sewer and Watermain

1. There appears to be a conflict between the storm sewer and sanitary sewer service to the assisted senior living facility.

2. Provide 4" thickness of insulation at watermain and storm sewer crossings where storm sewer and drainage structures provide less than 3.5 feet of cover (not 2.0') over the watermain.
3. Verify an 8" watermain as proposed will provide adequate fire flow and pressure to serve the number of units and building elevations being proposed.
4. Water and sanitary sewer connections to buildings must conform to the plumbing code as interpreted by the local building officials.
5. The applicant shall include a drain tile system behind the curbs to convey sump pump discharge from the units as detailed in the City Standard Specifications.
6. The proposed 24" pipe between Ex. Structure 16 and MH 1 appears to be oversized assuming it is proposed to take the existing 9" line from the west and the proposed 10" line from the development.
7. Due to the depth, this pipe should be shown as SDR 26 with the construction plans and the adjacent storm sewer should be shown as being impacted with construction.
8. Provide for granular bedding for installed utility pipe in unsuitable materials per the applicable City standard detail plate.
9. Show and call out the size and material of the existing waterlines where the development is connecting on the west and south.
10. The Independent Senior Living facility is shown with an 8" DIP service while the other facilities were shown with 6" DIP services. Was this done by design?
11. Provide 45-degree bends in lieu of 90-degree bends near the western connection with the existing watermain.
12. Place a hydrant at the original proposed location along Kenzie Terrace near the connection to the existing watermain on the south side of the development. The main can be capped/plugged at that point.

Detail Sheet

1. Include all applicable standard detail plates from the most recent edition (2018) of the City of St. Anthony's General Specifications and Standard Detail Plates for Street and Utility Construction for project construction.

Thank you for the opportunity to provide comments on this project. If you have any questions, please do not hesitate to contact me at 763-287-7182.

Sincerely,
WSB & Associates, Inc.



Todd Hubmer, PE
City Engineer



Memorandum

To: *Breanne Rothstein*

From: *Joey Abramson*

Cc: *Todd Hubmer*

Date: *February 16, 2018*

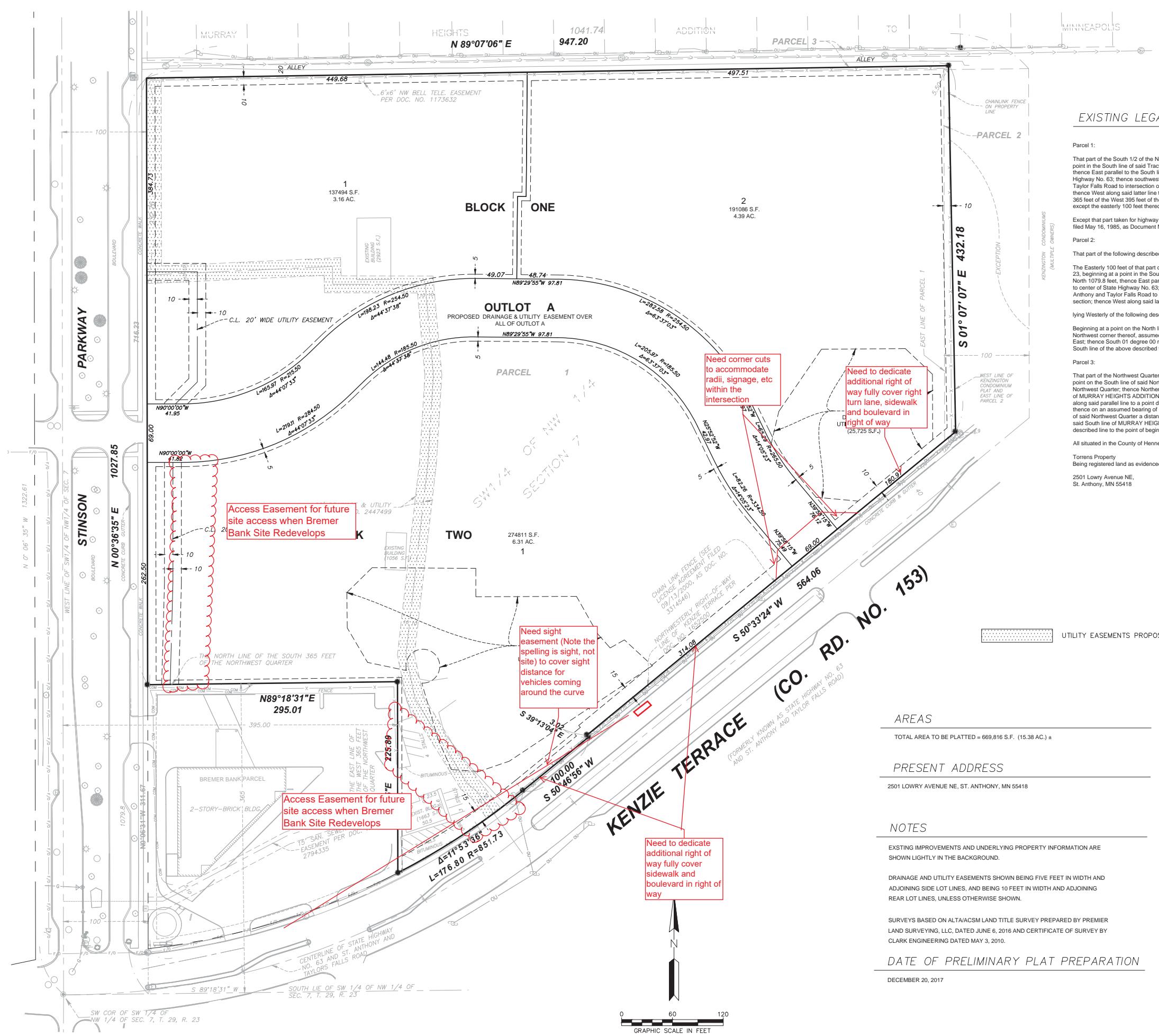
Re: *The Village LLC Development Stormwater Management Review*

WSB has reviewed the stormwater management memorandum produced by Wenck for the above project dated January 26, 2018.

The review has found that the project complies with applicable freeboard and rate control stormwater management regulations. The applicant needs to provide updated information related to water quality treatment. See comments below.

Comments on the stormwater management plan include the following:

- Ponds should be labeled on the plans to correspond to the IDs in the report and models: North Pond should include "1P" and South Pond should include "2P"
- The EOF for Pond 2P (south) is noted to be "18.90" on the plans, but is noted as 919.0 in the stormwater management report.
- The report notes that the proposed development will achieve 65% TP loading reduction (page 1) but the table indicates that 72% removals will be achieved. Revise for consistency.
- It should be considered that unless the BayFilter is capable of removing dissolved phosphorus from stormwater, its efficacy will likely be reduced from 50% removal if it is downstream of the sedimentation ponds. This is because those ponds already remove a large portion of the particulate phosphorus. Applicant must provide clarification on the BayFilter removal abilities or provide updated calculations for removals.



THE VILLAGE LLC DEVELOPMENT

THE VILLAGE LLC DEVELOPMENT

ST. ANTHONY VILLAGE, MINNESOTA

Prepared for:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

Prepared for:
THE VILLAGE LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

I HEREBY CERTIFY THAT THIS PLAN
SPECIFICATION, OR REPORT WAS
PREPARED BY ME OR UNDER MY
DIRECTION, SUPERVISION, AND IS A
DULY REGISTERED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE
STATE OF MINNESOTA.

License #:
Date:

Project #:
5005-0003

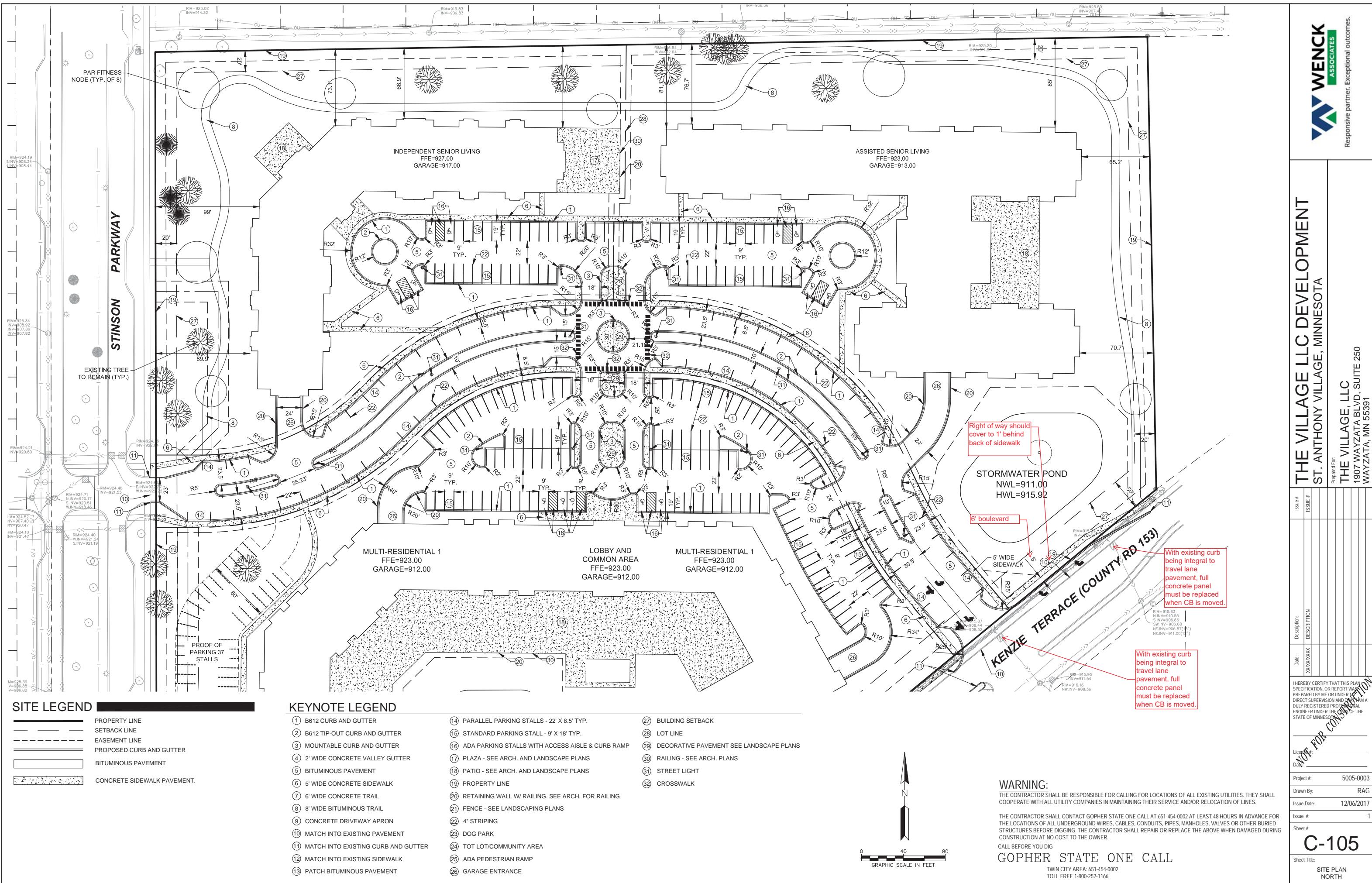
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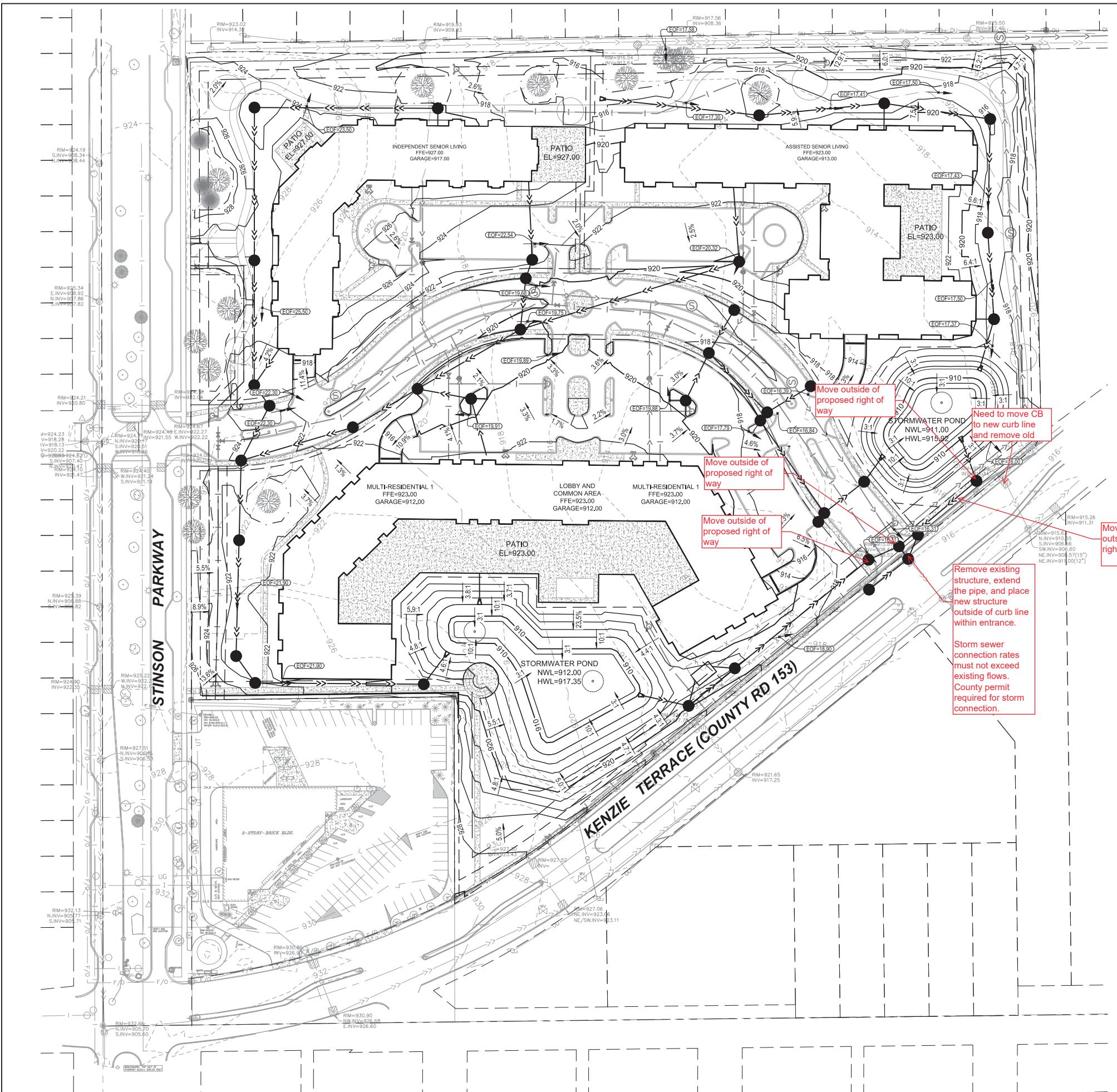
Issue Date:
12/06/2017

Issue #:
1

Sheet #:
C-101

Sheet Title:
PRELIMINARY PLAT





GRADING LEGEND

CONSTRUCTION LIMITS
PROPERTY LINE
SETBACK LINE
EASEMENT LINE
EXISTING CONTOUR
PROPOSED CONTOUR
PROPOSED STORM SEWER
PROPOSED CATCH BASIN
PROPOSED MANHOLE
PROPOSED SPOT ELEVATION (ALL ELEVATIONS ARE TO FLOWLINE UNLESS OTHERWISE NOTED)
FINISHED GRADE SPOT ELEVATION
MATCH EXISTING SPOT ELEVATION
TOP OF CURB SPOT ELEVATION
HIGH POINT SPOT ELEVATION
EMERGENCY OVERFLOW SPOT ELEVATION
TOP OF WALL SPOT ELEVATION
BOTTOM / TOE OF WALL SPOT ELEVATION
PROPOSED RIDGE / VALLEY / BREAKLINE
EOF FLOW ARROW
PROPOSED SLOPE ARROW
EXISTING STORM STRUCTURE
EXISTING STORM SEWER

NOTE: SEE C-501 STORM SEWER PLAN FOR STORM SEWER DESIGN DETAILS

THE VILLAGE LLC DEVELOPMENT
ST. ANTHONY VILLAGE, MINNESOTAPrepared For:
THE VILLAGE, LLC
1907 WAYZATA BLVD, SUITE 250
WAYZATA, MN 55391

Date:	Issue #
xxxx/xxxx	xxxx

I HEREBY CERTIFY THAT THIS PLAN
SPECIFICATION, OR REPORT WAS
PREPARED BY ME OR UNDER
MY DIRECTION AND THAT I AM A
DULY REGISTERED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE
STATE OF MINNESOTA.

License #:
Date:

Project #:
5005-0003

Drawn By:
RAG

Issue Date:
12/06/2017

Issue #:
1

Sheet #:
C-301

Sheet Title:
GRADING
PLAN

WARNING:

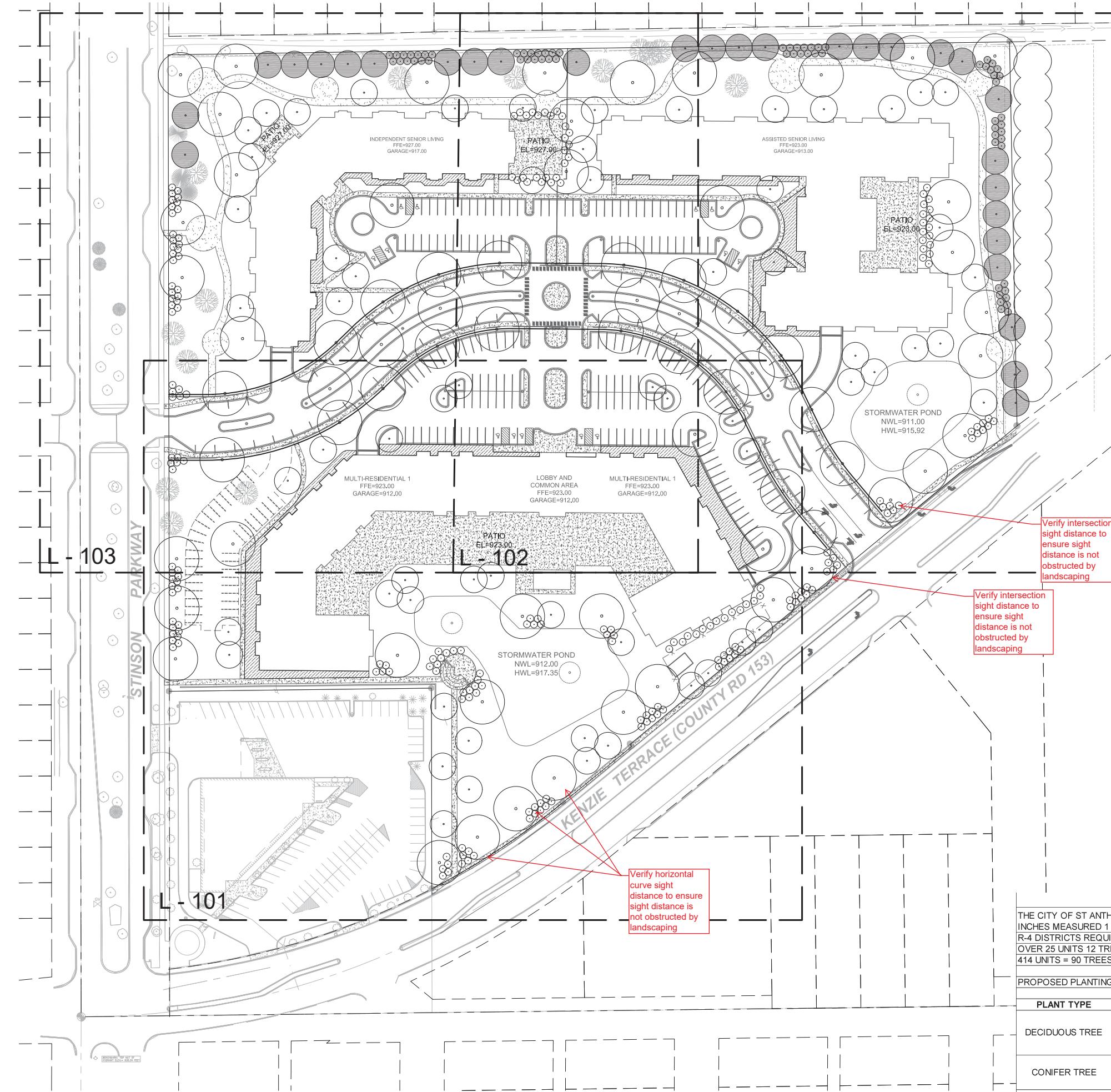
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CALL BEFORE YOU DIG

GOPHER STATE ONE CALL
TWIN CITY AREA: 651-454-0002
TOLL FREE 1-800-252-1166

0 60 120
GRAPHIC SCALE IN FEET



PLANTING LEGEND

- OVERSTORY TREE
- ORNAMENTAL TREE
- CONIFEROUS TREE
- CONIFEROUS SHRUB
- DECIDUOUS SHRUB
- EXISTING TREE



Responsive partner. Exceptional outcomes.

THE VILLAGE LLC DEVELOPMENT
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5005-0003

Drawn By:
RAG

Issue Date:
12/06/2017

Issue #:
1

Sheet #:
L-100

Sheet Title:
PLANTING PLAN -
OVERALL CONCEPT

PLANTING SCHEDULE

THE CITY OF ST ANTHONY REQUIRES NO LESS THAN THE FOLLOWING NUMBER OF TREES HAVING A DIAMETER OF AT LEAST 1-3/4 INCHES MEASURED 1 FOOT ABOVE THE GROUND WILL BE PLANTED IN THE DEVELOPMENT OF ANY SITE WITHIN THE CITY
R-4 DISTRICTS REQUIRE AT LEAST THE FOLLOWING NUMBER OF TREES PER BUILDING
OVER 25 UNITS 12 TREES, PLUS 1 TREE FOR EVERY 5 UNITS OVER 25.
414 UNITS = 90 TREES

PROPOSED PLANTINGS:

PLANT TYPE	SYMBOL	QTY	COMMON NAME	SCIENTIFIC NAME	HEIGHT	WIDTH	SPACING	SIZE
DECIDUOUS TREE	Af	27	AUTUMN BLAZE MAPLE	<i>Acer x freemanii 'Jeffersred'</i>	50'	40'	PER PLAN	2" CAL.
	Co	18	HACKBERRY	<i>Celtis occidentalis</i>	50'	50'	PER PLAN	2" CAL.
	Qb	22	SWAMP WHITE OAK	<i>Quercus bicolor</i>	50'	50'	PER PLAN	2" CAL.
CONIFER TREE	Ac	13	CONCOLOR FIR	<i>Abies concolor</i>	30'	30'	PER PLAN	6'
	Pg	14	BLACK HILLS SPRUCE	<i>Picea glauca densata</i>	40'	30'	PER PLAN	6'
ORNAMENTAL TREE	N/A	69	CRABAPPLE/LILAC					
		163			20'	20'	PER PLAN	2" CAL.
			TOTAL					

Breanne Rothstein
City of Saint Anthony Village
3301 Silver Lake Road
St. Anthony, MN 55418
planner@savmn.com

Members of the St. Anthony City Council and Planning Commission,

St. Anthony is a great place to live, work and play. Part of making St. Anthony a better community is planned growth. As residents we want the community to grow and thrive; we just want it to be done in a way that protects our community, our environment and our quality of life.

I grew up in this City and now as a baby boomer returned to settle in as the golden years approach. I am a second-generation resident/owner of the Kenzington Condominium and concerned with the proposed development by The Village, LLC. The development proposes the building 70 feet from my unit. As my unit and the other 40 units on the west side of the building, the proposed alignment will leave many homeowners in a very dark mood. Dating back to the 19th century, there were laws to prevent a neighbor from going ahead with construction if it blocks out sunshine. It is frustrating to see that that this could happen to us. I would hope as residents of St. Anthony yourself, that you would give consideration to the location of the building and the impact it would have to block light. St. Anthony is known for its suburban structure not urban. A community where you can enjoy more space and have a bigger area for a backyard or garden.

Not to mention the value of our property of the building significantly altering our view. This proposed development is set to deliberately block our views. A blocked view would make the home worth less and we all want increased property value not decreased.

Thank you for considering all aspects of this proposed development carefully in the best interest for the City and its current and future residents.

Karen Hanson
2601 Kenzie Terrace #417
St. Anthony, MN 55418

kmhanson074@gmail.com
651-485-6011